

CITY OF SPARKS 2020 CDBG STREET IMPROVEMENTS

FEDERALLY FUNDED

D ST. FROM ROCK BLVD TO 15TH ST.,

AND 16TH ST. FROM D ST. TO PRATER WAY

WASHOE COUNTY, NEVADA

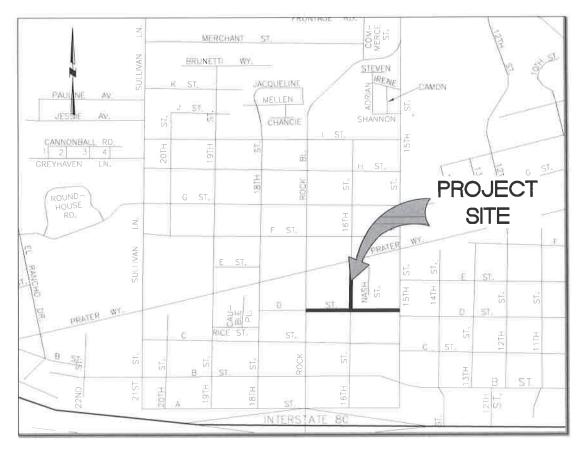
COS PROJECT NUMBER: 19/20-020 PWP NO: WA-2020-146

SPARKS CITY COUNCIL

MAYOR RONALD E. SMITH
WARD ONE DONALD ABBOTT
WARD TWO ED LAWSON
WARD THREE PAUL ANDERSON
WARD FOUR CHARLENE BYBEE
WARD FIVE KRISTOPHER DAHIR
CITY MANAGER NEIL C. KRUTZ

CIVIL:

CFA, INC. 1150 CORPORATE BOULEVARD RENO, NEVADA 89502 (775) 856-1150 VOICE (775) 856-1160 FAX CONTACT: MIKE WILHELM, P.E.



VICINITY MAP



APPROVALS

JON R. ERICSON P.E., P.T.O.E. CITY ENGINEER 2/3/2020

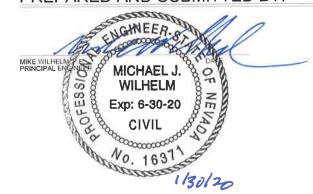
BASIS OF BEARINGS AND COORDINATES:

THE BASIS OF BEARINGS FOR THIS SURVEY IS NEVADA STATE PLANE, WEST ZONE NADB3(94) BASED ON REAL TIME KINEMATIC (RTK) GPS OBSERVATIONS UTILIZING CORRECTIONS FROM THE NORTHERN NEVADA COOPERATIVE REAL TIME NETWORK COORDINATES AND DISTANCES HEREON ARE AT GROUND LEVEL BASED ON A COMBINED GRID TO GROUND FACTOR OF 1.000197939.

BASIS OF ELEVATIONS:

THE BASIS OF ELEVATIONS FOR THIS SURVEY IS THE NORTH AMERICAN VERTICAL DATUM OF 1986 (NAVD86) BASED UPON GPS OBSERVATIONS UTILIZING GEOID99, CITY OF SPARKS BENCHMARK #59 AS USED IN THIS SURVEY WITH AN ELEVATION OF 4422 01 FEET.

PREPARED AND SUBMITTED BY:



SPECIFICATIONS REFERENCED HEREIN REFER TO THE SSPWC UNLESS INDICATED OTHERWISE. 2. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN ALL NECESSARY TRAFFIC CONTROL, THROUGHOUT CONSTRUCTION, IN ACCORDANCE WITH APPLICABLE PARTS OF SPECIFICATION SECTION 332, AND

THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, (MUTCD, LATEST EDITION).

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE GENERAL SAFETY DURING CONSTRUCTION, AND ALL WORK SHALL CONFORM TO PERTINENT SAFETY REGULATIONS AND CODES. THE CONTRACTOR SHALL FENCE AND OR BARRICADE THE CONSTRUCTION AREA AS REQUIRED TO PROTECT ADJACENT SITES, VEHICULAR TRAFFIC, AND PEDESTRIAN TRAFFIC. THE CONTRACTOR SHALL BE SOLELY AND COMPLETELY RESPONSIBLE FOR COMPLIANCE WITH ALL APPLICABLE PROVISIONS OF OSHA AND NRS CHAPTER 618, IN THE CONSTRUCTION PRACTICES FOR ALL EMPLOYEES DIRECTLY ENGAGED IN THE CONSTRUCTION OF THIS PROJECT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION AND/OR PROTECTION OF ALL EXISTING AND PROPOSED PIPING, UTILITIES, TRAFFIC SIGNAL EQUIPMENT (BOTH ABOVE GROUND AND BELOW GROUND), STRUCTURES ADJACENT TO STREETS, AND ALL OTHER EXISTING IMPROVEMENTS THROUGHOUT CONSTRUCTION.

5. THE CONTRACTOR SHALL PURSUE WORK IN A CONTINUOUS AND DILIGENT MANNER TO ENSURE A TIMELY COMPLETION OF THE PROJECT.

ALL CONSTRUCTION SHALL BE CLOSELY COORDINATED WITH THE CITY OF SPARKS SO THAT THE QUALITY OF WORK CAN BE CHECKED FOR APPROVAL.

7 THE CONTRACTOR SHALL INCORPORATE ADEQUATE DRAINAGE PROCEDURES DURING THE CONSTRUCTION PROCESS TO ELIMINATE EXCESSIVE PONDING AND/OR EROSION.

8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE SITE IN A NEAT AND ORDERLY MANNER THROUGHOUT THE CONSTRUCTION PROCESS. ALL MATERIALS SHALL BE STORED WITHIN APPROVED CONSTRUCTION AREAS.

9 THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTINUOUS DUST CONTROL THROUGHOUT THE CONSTRUCTION OF ALL ITEMS SHOWN ON THESE DRAWINGS IN CONFORMITY WITH SECTION 40.030 OF WASHOE COUNTY AIR POLLUTION REGULATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REGULAR CLEANING OF ALL MUD, DIRT, DEBRIS, ETC., FROM ANY AND ALL ADJACENT STREETS AND SIDEWALKS.

10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING, ALL PERMITS AS REQUIRED BY CITY OF SPARKS. THE DUST CONTROL PERMIT SHALL BE AT THE CONTRACTOR'S EXPENSE. THE ENCROACHMENT, EXCAVATION, AND LANE CLOSURE PERMITS ARE "NO FEE" PERMITS FOR CITY

11. SPARKS MUNICIPAL CODE 20.04.005.D RESTRICTS CONSTRUCTION HOURS TO 7:00 A.M. UNTIL 7:00 P.M., MONDAY THROUGH FRIDAY AND 9:00 A.M. UNTIL 5:00 P.M. ON SATURDAY. FOR THIS PROJECT, THE DEFINED CONSTRUCTION HOURS WILL BE 7:00 A.M UNTIL 7:00 P.M., MONDAY THROUGH FRIDAY UNLESS OTHERWISE REQUIRED BY THESE SPECIFICATIONS OR REQUESTED BY THE CITY PROJECT COORDINATOR. THE CONTRACTOR SHALL NOT COMMENCE CONSTRUCTION OPERATIONS BEFORE SEVEN O'CLOCK (7:00 A.M. PACIFIC TIME) EACH WORKING DAY EXCEPT AS DIRECTED BY CITY PROJECT COORDINATOR AND AS SPECIFIED HEREIN.

THE CONTRACTOR SHALL NOT PERFORM ANY CONTRACT WORK ON SATURDAY, SUNDAY, LEGAL HOLIDAYS, AND OUTSIDE OF THE TWELVE (12) HOURS AVAILABLE DURING A REGULAR WORKING DAY EXCEPT AS DIRECTED AND APPROVED BY THE CITY PROJECT COORDINATOR AND AS SPECIFIED

WHEN DIRECTED TO WORK OUTSIDE OF THE LEGALLY PERMITTED CONSTRUCTION HOURS DEFINED ABOVE, THE CONTRACTOR SHALL FIRST OBTAIN APPROVAL FROM THE CITY PROJECT COORDINATOR AT LEAST SEVENTY-TWO (72) HOURS PRIOR TO COMMENCING SUCH OVERTIME WORK. IF THE CONTRACTOR PLANS TO PERFORM WORK ON SATURDAY OR SUNDAY, HE/SHE SHALL OBTAIN APPROVAL BY THE WEDNESDAY PRIOR TO WORK ON THE SATURDAY OR SUNDAY FOR WHICH WORK IS PLANNED, IF THE CONTRACTOR PLANS TO PERFORM WORK ON A LEGAL HOLIDAY, HE/SHE SHALL FIRST OBTAIN APPROVAL FROM THE CITY PROJECT COORDINATOR AT LEAST 72 HOURS IN

CONTRACTOR SHALL OBTAIN APPROVAL THROUGH THE TEMPORARY LISE PERMIT (TILP) AND PAY A CONTRACTOR SHALL OBTAIN APPROVAL INROUGH THE LEMPORARY USE PERMIT (1.0.P.) AND PAIL A FEE OF ONE HUNDRED (\$100.00) DOLLARS TO WORK OUTSIDE OF THE ABOVE LEGALLY PERMITTED CONSTRUCTION HOURS. THE REQUEST SHALL INCLUDE JUSTIFICATION OF HOW PUBLIC SAFETY WILL BE ENHANCED THROUGH WORKING OUTSIDE OF THE RESTRICTED CONSTRUCTION HOURS. SUBMITTAL AND REVIEWED OF THE RESTRICTED CONSTRUCTION HOURS. AND PAYMENT OF FEES DOES NOT GUARANTEE APPROVAL.

: CONTRACTOR SHALL BE CHARGED FOR ALL OF CITY OF SPARKS' EMPLOYEE(S) TIME SPEN FOR OVERTIME, SATURDAY, SUNDAY OR HOLIDAY WORK, BASED ON THE EMPLOYEE'S HOURLY RATE, PLUS BENEFITS. THE CONTRACTOR WILL BE NOTIFIED OF THE COSTS INCURRED AND IF THE PAYMENT IS NOT MADE, SUCH COSTS WILL BE DEDUCTED FROM ANY PAYMENT DUE TO THE CONTRACTOR IN ADDITION TO THE CHARGE FOR THE CITY EMPLOYEE(S) TIME OUTSIDE A STANDARD

12. ALL DIMENSIONS ARE TO FRONT FACE OF CURB UNLESS NOTED OTHERWISE.

13 SHOULD ANY PREHISTORIC OR HISTORIC REMAINS OR ARTIFACTS BE DISCOVERED DURING SITE DEVELOPMENT, WORK SHALL BE TEMPORARILY HALTED AT THE SPECIFIC SITE AND THE STATE HISTORIC PRESERVATION OFFICE OF THE DEPARTMENT OF MUSEUMS, LIBRARY AND ARTS, SHALL BE NOTIFIED TO RECORD AND PHOTOGRAPH THE SITE. THE PERIOD OF TEMPORARY DELAY SHALL BE LIMITED TO A MAXIMUM OF TWO (2) WORKING DAYS FROM THE DATE OF NOTIFICATION

14. A PRECONSTRUCTION CONFERENCE WILL BE HELD WITH CITY OF SPARKS PRIOR TO ANY

15. NO MATERIALS OF ANY KIND SHALL BE STOCKPILED OR CONSTRUCTION EQUIPMENT PARKED ON

16. CONSTRUCTION OF STREET IMPROVEMENTS MUST ALLOW FOR THE PERPETUATION OF ALL EXISTING LEGAL ACCESSES AND EXISTING DRIVEWAYS.

17. THE CONTRACTOR SHALL FURNISH PRODUCT SUBMITTALS FOR IMPROVEMENTS INCLUDING MANUFACTURER'S SPECIFICATIONS, SHOP DRAWINGS, CONCRETE MIX DESIGNS, ASPHALT MIX DESIGNS, GRADATION CERTIFICATIONS, ETC. FOR REVIEW BY THE ENGINEER OF RECORD. SUBMITTALS SHALL BE FURNISHED PROMPTLY AFTER EXECUTION OF THE CONTRACT, BUT IN ALL CASES PRIOR TO DELIVERY OF SUCH MATERIALS.

18. THE CONTRACTOR UPON COMPLETION OF THE PROJECT, SHALL PREPARE AND SUBMIT RECORD DRAWINGS TO THE OWNER. SUBMITTAL SHALL BE IN THE FORM OF A CD IN 300 DPI, MULTIPAGE PDF FORMAT.

19. THE CONTRACTOR IS REQUIRED TO FILE A NOTICE OF INTENT (NOI) WITH THE NEVADA DEPARTMENT . THE CONTRACTOR IS REQUIRED TO FILE A NOTICE OF INTENT (NOI) WITH THE NEVADA DEPARTMENT OF ENVIRONMENTAL PROTECTION. THE CONTRACTOR IS REQUIRED TO SUBMIT THE NOI, PAY THE ASSOCIATED FEES, AND PROVIDE A COPY OF THE NOI RECEIPT WITH THE EE PERMIT APPLICATION. BEST MANAGEMENT PRACTICES SHALL BE IN PLACE PRIOR TO START OF CONSTRUCTION. THE CONTACTOR SHALL COMPLY AND MAINTAIN THE CONSTRUCTION SITE INSPECTION CHECKLIST, LOCATED IN APPENDIX D OF THE TRUCKEE MEADOWS BMP HANDBOOK. THE CHECKLIST SHALL BE ONSITE FOR REVIEW BY STATE AND LOCAL GOVERNMENT INSPECTORS. THE CONTACTOR SHALL INSPECT THE SITE DAILY; PRIOR TO FORECASTED RAIN EVENTS AND WITHIN 24 HOURS OF ANY RAIN EVENT. IF AN INSPECTOR DETERMINES THAT INSTALLED STORM WATER CONTROLS PLACE THE CITY AT RISK OF VIOLATING ITS NPDES PERMIT. THE INSPECTOR MAY ORDER A CHANGE TO THE

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WOOD FENCE

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SANITARY SEWER FASEMENT

SPECIFICATIONS

STANDARD

TANGENT

SYMMETRICAL

THRUST BLOCK

TEMPERATURE

TOP OF WALL

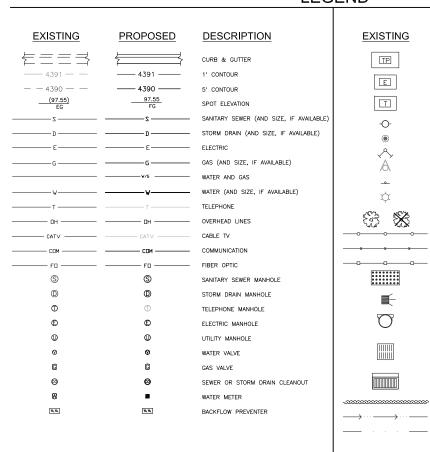
VERTICAL CURVE

TOP FACE

TYPICAL VELOCITY

VERTICAL VALLEY GUTTER

YARD



UTILITY NOTES **PROPOSED** DESCRIPTION

TRANSFORMER PAD

ELECTRIC VAULT

POWER POLE

FIRE HYDRANT

STREET SIGN

CHAINLINK FENCE

TRAFFIC SIGNAL

GRADE BREAK

FLOW LINE SWALE

BARBED WIRE FENCE

TELEPHONE VAULT

SURVEY MONUMENT

FIRE DEPARTMENT CONNECTION

TREE / TREE TO BE REMOVED

HANDICAP (ADA) RAMP DOMES

TYPE 1 STORM DRAIN CATCH BASIN

TYPE 3R STORM DRAIN CATCH BASIN

TYPE 4R STORM DRAIN CATCH BASIN

STREET LIGHTING AND/OR LIGHT POLE

 UTILITIES MAY EXIST THAT ARE NOT SHOWN ON THE PLANS. HORIZONTAL AND VERTICAL LOCATIONS
 OF EXISTING UTILITIES ARE APPROXIMATE ONLY. THE CONTRACTOR SHALL VERIFY ACTUAL
 LOCATIONS OF EXISTING UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL CALL UNDERGROUND SERVICES ALERT (USA) AT 811 PRIOR TO ANY EXCAVATION.

2. THE CONTRACTOR SHALL LOCATE ALL EXISTING UTILITIES WHICH MAY CONFLICT WITH THE NEW

3. THE CONTRACTOR SHALL NOTIFY NV ENERGY AND TMWA AT LEAST 48 HOURS PRIOR TO THE START OF UTILITY CONSTRUCTION.

4. THE CONTRACTOR SHALL MAKE EVERY ATTEMPT TO AVOID SHUT OFF OR DISCONNECTION OF ACTIVE UTILITIES. IF SHUT OFFS ARE UNAVOIDABLE, THE CONTRACTOR SHALL COORDINATE THAT SHUT OFF WITH THE AFFECTED CUSTOMERS AND UTILITY COMPANIES AT LEAST 48 HOURS IN ADVANCE OF THE DISRUPTION OF THE SERVICE.

5. A MANHOLE PROTECTION PLAN SHALL BE SUBMITTED AND APPROVED PRIOR TO ANY MANHOLE ADJUSTMENTS. THE PLAN SHALL CLEARLY IDENTIFY HOW THE CONTRACTOR WILL PROTECT FROM ALL DEBRIS ENTERING INTO THE SYSTEM AND A DETAIL OF HOW THE CONTRACTOR IS PREPARED FOR EMERGENCY OVERFLOWS. PRIOR TO PERFORMING ANY ADJUSTMENTS OR GROUTING, THE ENGINEER OF RECORD SHALL OBSERVE AND VERIFY THAT THE CONTRACTOR IS IN COMPLIANCE WITH THE "MANHOLE PROTECTION PLAN".

MANHOLES AND CATCH BASINS SHALL BE PORTLAND CEMENT CONCRETE PER SPECIFICATION SECTION 204.00. MANHOLES AND CATCH BASINS SHALL BE PLACED PER APPLICABLE PARTS OF SPECIFICATION SECTIONS 305.00, 306.00, AND 311.00.

ALL CATCH BASIN COVERS DESIGNATED TO BE RESET TO GRADE SHALL BE REPLACED WITH A NEW FRAME AND COVER AT THE CONTRACTOR'S EXPENSE.

8. CONTRACTOR TO VACUUM CLEAN ALL STORM DRAIN & SANITARY SEWER MANHOLES AND CATCH BASINS AFTER REMOVING CONTAINMENT DEVICES UPON COMPLETION OF THE PROJECT

9. ALL BMPS SHALL BE REMOVED FROM CATCH BASINS & MANHOLES PRIOR TO FINAL ACCEPTANCE

10 THE CONTRACTOR SHALL POTHOLE ALL WATER MAINS AHEAD OF CONSTRUCTION AND VERIEY THAT THE CONTRACTOR SHALL POTHOLE ALL WATER MAINS AHEAD OF CONSTRUCTION AND VERTIT THAT SUFFICIENT VERTICAL CLEARANCE EXISTS TO PROVIDE SUFFICIENT PROTECTIVE COVER AND TO ACCOMMODATE THE STREET STRUCTURAL SECTION. IF WATER PIPE IS NEAR THE SUBGRADE, THE CONTRACTOR MAY NEED TO PERFORM ADDITIONAL WORK TO SHIELD WATER MAINS FROM DAMAGE. SUCH WORK MAY INCLUDE, BUT NOT BE LIMITED TO, REDUCING EXCAVATION/PULVERIZATION DEPTH-OVER THE PIPE ZONE, ADDING BASE OVER THE PIPE TO INCREASE COVER PRIOR TO COMPACTION, OR REDUCING VIBRATORY COMPACTION AND CONSTRUCTION TRAFFIC OVER THE PIPE ZONE. ACTUAL FIELD CONDITIONS SHOULD BE EXAMINED AND CONSTRUCTION METHODS DISCUSSED PRIOR TO BEGINNING EXCAVATION. WATER MAINS IN 1 STREET AND 16TH STREET ARE DI CL 350 PSI, AND D STREET IS C-900 PVC CL 235 PSI.

11. TMWA WILL INSPECT, CLEAN, OPERATE AND DOCUMENT ALL TMWA—OWNED VALVES WITHIN THE CONSTRUCTION ZONE PRIOR TO THE PRE—CONSTRUCTION MEETING. THE CONTRACTOR SHALL REVIEW AND VERIFY THIS INFORMATION WITH CITY OF SPARKS. ANY DISCREDAYCY SHALL BE BROUGHT TO THE ATTENTION OF TMWA'S INSPECTOR PRIOR TO CONSTRUCTION. THE FINAL LIST WILL BE USED FOR VALVE ADJUSTMENT/CLEANING VERIFICATION AT THE END OF THE PROJECT.

12 STEEL PLATES - THE LISE OF STEEL PLATES SERVING AS A TEMPORARY TRENCH COVER SHALL ONLY BE USED WITHIN CLOSED TRAVEL LANES. THE USE OF STEEL PLATES WITHIN CLOSED TRAVEL LANES. THE USE OF STEEL PLATES WITHIN CLOSED TRAVEL LANES SERVING AS A TEMPORARY TRENCH COVER SHALL HAVE ASPHALT RAMPS CONSTRUCTED AROUND THE PERIMETER OF THE PLATE(S) FOR EMERGENCY VEHICLES AND CONSTRUCTION VEHICLES. WARNING SIGNS MUST BE APPROPRIATELY PLACED TO CAUTION MOTORISTS. STEEL PLATES WILL NOT BE ALLOWED IF SNOW OR FREEZING TEMPERATURES ARE

ABBREVIATIONS

AB	ANCHOR BOLT	E	EASTING	MDD
ABAN	ABANDONED	EX or EXIST		MECH
ABC	AGGREGATE BASE COURSE	EA	EACH	MFR
AC	ASPHALT CONCRETE	EC	END CURVE	MH
ADD	ADDITIONAL	ECR	END CURB RETURN	MIN
ADJ	ADJACENT	ECC	ECCENTRIC	MISC
APPROX	APPROXIMATE	EL	ELEVATION	MPH
APPR	APPROVED	ELEC	ELECTRIC	N
ARV	AIR RELEASE VALVE	ENGR	ENGINEER	NDP
BC	BEGIN CURVE	EVC	END OF VERTICAL CURVE	NTS
BCR	BEGIN CURB RETURN	EW	EACH WAY	OC
BFC	BACK FACE OF CURB	EXP JT	EXPANSION JOINT	OG
BLDG	BUILDING	FES	FLARED END SECTION	OZ
BM	BENCH MARK	FFC	FRONT FACE OF CURB	PC
BOT	BOTTOM	FG	FINISH GRADE	PCC
BSW	BACK OF SIDEWALK	FH	FIRE HYDRANT	PI
BVC	BEGIN VERTICAL CURVE	FIG	FIGURE	PL or i
CB	CATCH BASIN	E or FL	FLOWLINE	PRC
CFM	CUBIC FEET PER MINUTE	FPS	FEET PER SECOND	PREFA
CFS	CUBIC FEET PER SECOND	FT	FOOT or FEET	PROP
CI	CAST IRON	F	DEGREE FAHRENHEIT	PSF
CJ	CONSTRUCTION JOINT	G	GAS	PSI
⊈ or CL	CENTERLINE	GAL	GALLON	PT
CLR	CLEAR	GALV	GALVANIZED	PUE
CMP	CORRUGATED METAL PIPE	GB	GRADE BREAK	PVC
CMU	CONCRETE MASONRY UNIT	GPD	GALLONS PER DAY	PVI
CO	CLEANOUT	HORIZ	HORIZONTAL	PVMT
CONC	CONCRETE	HP	HORSEPOWER	R
CONN	CONNECTION	ID	INSIDE DIAMETER	RCB
CONT	CONTINUOUS	ΙE	INVERT ELEVATION	RCP
COORD	COORDINATE	IN	INCH	RD
CTR	CENTER	INV	INVERT	REF
CU	CUBIC	IRR	IRRIGATION	REINF
CU FT	CUBIC FEET	KW	KILOWATT	REQD
CU IN	CUBIC INCH	L	LENGTH	RT
CU YD	CUBIC YARD	LAT	LATERAL	RW or
CULV	CULVERT	LB	POUNDS	SCH
DBL	DOUBLE	LB/CU FT	POUNDS PER CUBIC FOOT	SD
DEP	DEPRESSED	LONG	LONGITUDINAL	SECT
DTL	DETAIL	LT	LEFT	SF
DI	DROP INLET	MAX	MAXIMUM	SI
DIA	DIAMETER			
DWG	DRAWING			

UTILITY OWNERSHIP

SANITARY SEWER & STORM DRAIN

WATER

GAS & ELECTRIC

TELEPHONE.

CABLE TV

MAXIMUM DRY DENSITY MECHANICAL MANUFACTURER SPEC MANHOLE MINIMUM MISCELLANEOUS NORTHING SYMM NO DIRECT PAYMENT NOT TO SCALE ON CENTER ORIGINAL GROUND OUNCE POINT OF CURVE PORTLAND CEMENT CONCRETE
POINT OF INTERSECTION PROPERTY LINE POINT OF REVERSE CURVE PREFABRICATED PROPERTY
POUNDS PER SQUARE FOOT POUNDS PER SQUARE INCH POINT OF TANGENCY PUBLIC UTILITY EASEMENT POINT OF VERTICAL INTERSECTION PAVEMENT RADIUS REINFORCED CONCRETE BOX CULVERT REINFORCED CONCRETE PIPE REFERENCE or REFER

REINFORCED

REQUIRED STORM DRAIN SECTION SQUARE FOOT

CITY OF SPARKS

TMWA

AT&T

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WATER
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Call before you dig.

CAUTION - NOTICE TO CONTRACTOR 1. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AND FEATURES SO SHOWN ON THESE FLAYS IS BASED ON THE BEST INFORMATION AVAILABLE TO THE ENGINEER. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE.

2. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THESE LOCATIONS AND/OR ELEVATIONS AT THE PROPOSED POINTS OF CONNECTION AND IN AREAS OF POSSIBLE CONFLICT PRIOR TO BEGINNING CONSTRUCTION. SHOULD THE CONTRACTOR FIND ANY DISCREPANCES BETWEEN THE CONDITIONS EXISTING IN THE FIELD AND THE INFORMATION SHOWN ON THESE DRAWINGS, HE SHALL NOTIFY THE ENGINEER BEFORE PROCEEDING WITH CONSTRUCTION.

3. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO REMOVE OR RELOCATE ALL EXISTING UTILITIES AND FEATURES WHICH CONFLICT WITH THE PROPOSED IMPROVMEMENTS SHOWN ON THE PLANS. CONTRACTOR SHALL OBTAIN APPROVALS FROM THE COVERNING AGENCIES, THE ENGINEER, AND THE UTILITY COMPANIES PRIOR TO SUCH REMOVAL AND/OR RELOCATION.

 THE CONTRACTOR ASSUMES ALL RISK FOR ANY CONSTRUCTION PERFORMED WITH PRELIMINARY OR NONAPPROVED PLANS. 5. CONTRACTOR TO PROVIDE TRAFFIC CONTROL IN CONFORMANCE WITH THE LATEST EDITION OF MUTCD WHENEVER CONSTRUCTION IS IN PROGRESS WITHIN THE PUBLIC TRAVEL WAY.

CONCRETE & PAVEMENT NOTES

- FIBER-REINFORCED PORTLAND CEMENT CONCRETE (PCC) SHALL MEET THE REQUIREMENTS OF SPECIFICATION SECTION 202.00. PCC EXPOSED TO FREEZE-THAW ENVIRONMENTS SHALL HAVE A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 4,000 PSI AND MEET THE SPECIFIC REQUIREMENTS OF SPECIFICATION SECTION 337.10.01.01. CEMENT SHALL BE TYPE II ALL CEMENT CONCRETE SHALL HAVE A COARSE AGGREGATE GRADATION CONFORMING TO SIZE NO. 67. POLYPROPYLENE OR CELLULOSE FIBERS SHALL BE ADDED TO THE PCC AT 1.5 LBS. PER CUBIC YARD. ALL MATERIALS SHALL CONFORM TO
- 2. CURB. GUTTER AND SIDEWALK SHALL BE PLACED IN ACCORDANCE WITH SPECIFICATION SECTION 312.00.
- 3. REFERENCE DETAIL 2, SHEET C3.1 FOR ASPHALTIC CONCRETE REQUIREMENTS.
- 4. ASPHALT CONCRETE PAVEMENT SHALL BE PLACED IN ACCORDANCE WITH SPECIFICATION SECTION 320.00.
- 5 TEMPORARY PATCHES SHALL BE HOT-MIX ASPHALT WITH A MINIMUM OF 3-INCHES IN DEPTH LINLESS INDICATED TEMPORARY PATCHES SHALL BE HOT-MIX ASPHALT WITH A MINIMUM OF 3-INCHES IN DEPTH UNLESS INDICATED OTHERWISE. TEMPORARY PATCHES SHALL BE PLACED IMMEDIATELY FOLLOWING BACKFILLING AND COMPACTION OF TRENCHES THROUGH EXISTING PAVEMENTS. COLD MIX PATCHES SHALL ONLY BE ALLOWED ON AN INTERIM BASIS IF HOT-MIX PAVEMENT IS NOT AVAILABLE LOCALLY. COLD MIX PATCHES SHALL BE REPLACED WITH HOT-MIX PAVEMENT WHEN AVAILABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF TEMPORARY COLD MIX PATCHES UNTIL THE TIME THE TEMPORARY HOT MIX PATCHES ARE PLACED. COLD MIX PATCHES SHALL BE DURA PATCH AS REQUIRED BY THE CITY OF SPARKS. THE CONTRACTOR SHALL MAINTAIN PAVEMENT IN A SAFE AND SMOOTH CONDITION UNTIL FINAL PAVEMENT CAN BE PLACED, TO THE SATISFACTION OF THE ENGINEER.

DEMOLITION NOTES

- THE REMOVAL OF EXISTING IMPROVEMENTS SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL TECHNICAL SPECIFICATIONS AND THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION (SSPWC, 2012), SECTION 301.00.
- 2. NOT ALL EXISTING IMPROVEMENTS. ADJACENT PROPERTY. INCLUDING BUT NOT LIMITED TO NOT ALL EXISTING IMPROVEMENTS, ADJACENT PROPERTY, INCLUDING BUT NOT LIMITED TO, FENCING, WALLS, UTILITIES AND OTHER FACILITIES, MAILBOXES, WATER METERS, POWER POLES, OVERHEAD LINES, STREET SIGNS, CATCH BASINS, TREES AND PLANTS, ETC., ARE SHOWN ON THE PLANS. ALL EXISTING ITEMS THAT ARE NOT TO REMOVED SHALL BE PROTECTED FROM INJURY OR DAMAGE RESULTING FROM THE CONTRACTORS OPERATIONS IN ACCORDANCE WITH SPECIFICATION SECTION 301.04 AND 300.04.
- THE CONTRACTOR SHALL ADJUST ALL EXISTING MANHOLE RIMS, WATER & GAS VALVE COVERS, & UTILITY BOXES AND FRAME AND COVERS, BOTH HORIZONTALLY AND VERTICALLY, AS REQUIRED TO FIT THE NEW WORK. THE CONTRACTOR SHALL REPLACE ANY APPURTENANCE DAMAGED DURING RELOCATION AT THE COST OF THE CONTRACTOR.
- ALL EXISTING FEATURES IN CONFLICT WITH PROPOSED SHALL BE REMOVED, RELOCATED AND/OR REPLACED. CONTRACTOR SHALL OBTAIN APPROVAL FROM THE ENGINEER AND THE GOVERNING AGENCIES PRIOR TO SUCH REMOVAL, RELOCATION AND/OR REPLACEMENT.
- 5 ALL SETTLING DRAINAGE ISSUES AND UNSUITABLE MATERIALS SHALL BE REMOVED. CORRECTED, AND REPLACED WITH RECYCLED AGGREGATE BASE OR IMPORTED TYPE 2, CLASS B AGGREGATE BASE.
- 6. LENGTHS SHOWN FOR RAMPS ON DRIVEWAYS AND PEDESTRIAN RAMPS ARE FOR BID PURPOSES ONLY. LENGTHS SHALL BE VERIFIED IN FIELD AND CONFORM TO THE DETAILS AND SPECIFICATIONS PROVIDED.
- 7. LINE AND GRADE FOR IMPROVEMENTS SHALL BE DETERMINED IN THE FIELD BY THE CONTRACTOR IN COORDINATION WITH THE CITY OF SPARKS INSPECTOR

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> IOB NO 17-107-0 DESIGNED BY BEH/C HECKED BY DATE 01/21/2020

WILHELM

Exp. 06-30-2

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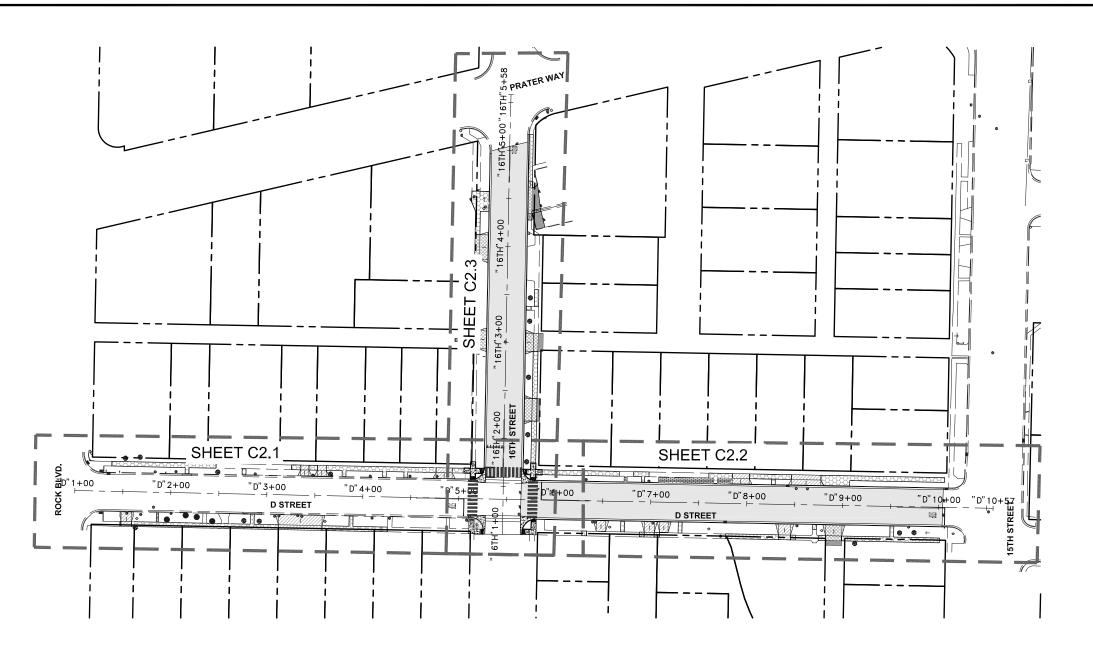
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1/30/20

S SHEET $\overline{\mathbf{m}}$ 9

ARNING: WRITTEN CONSENT IS REQUIRED OF CFA, OWNER OF DESIGNS & DRAWINGS AS INSTRUMENTS OF SERVICE, FOR DUPLICATION AND-OR DISTRIBUTION OF DOCUMENTS

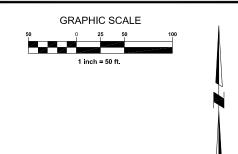
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SHEET INDEX - D STREET & 16TH STREET

SCALE: 1" = 50'

ARRING: WRITTEN CONSENT IS REQUIRED OF CFA, OWNER OF DESIGNS & DRAWINGS AS INSTRUMENTS OF SERVICE, FOR DUPLICATION AND OR DISTRIBUTION OF DOCUMENTS.



SHEET INDEX

C1.0 C1.1	TITLE SHEET GENERAL NOTES
C2.0	SHEET INDEX
C2.1	PLAN & PROFILE - D STREET, STA: "D" 1+00 - 6+00
C2.2	PLAN & PROFILE - D STREET, STA: "D" 6+00 - 10+57
C2.3	PLAN & PROFILE - 16TH STREET, STA: "16TH" 1+00 - 5+58
C3.0	DETAILS
C3.1	DETAILS
00.0	DETAILS



- SHEET INDEX

CITY OF SPARKS 2020 CDBG STREET IMPROVEMENTS PROJECT

SITE PLAN

JOB NO. 17-107-00 DESIGNED BY BEH/CJ HECKED BY 01/21/2020

SET DATE SHEET C2.0 BID 9

Know what's below. Call before you dig.

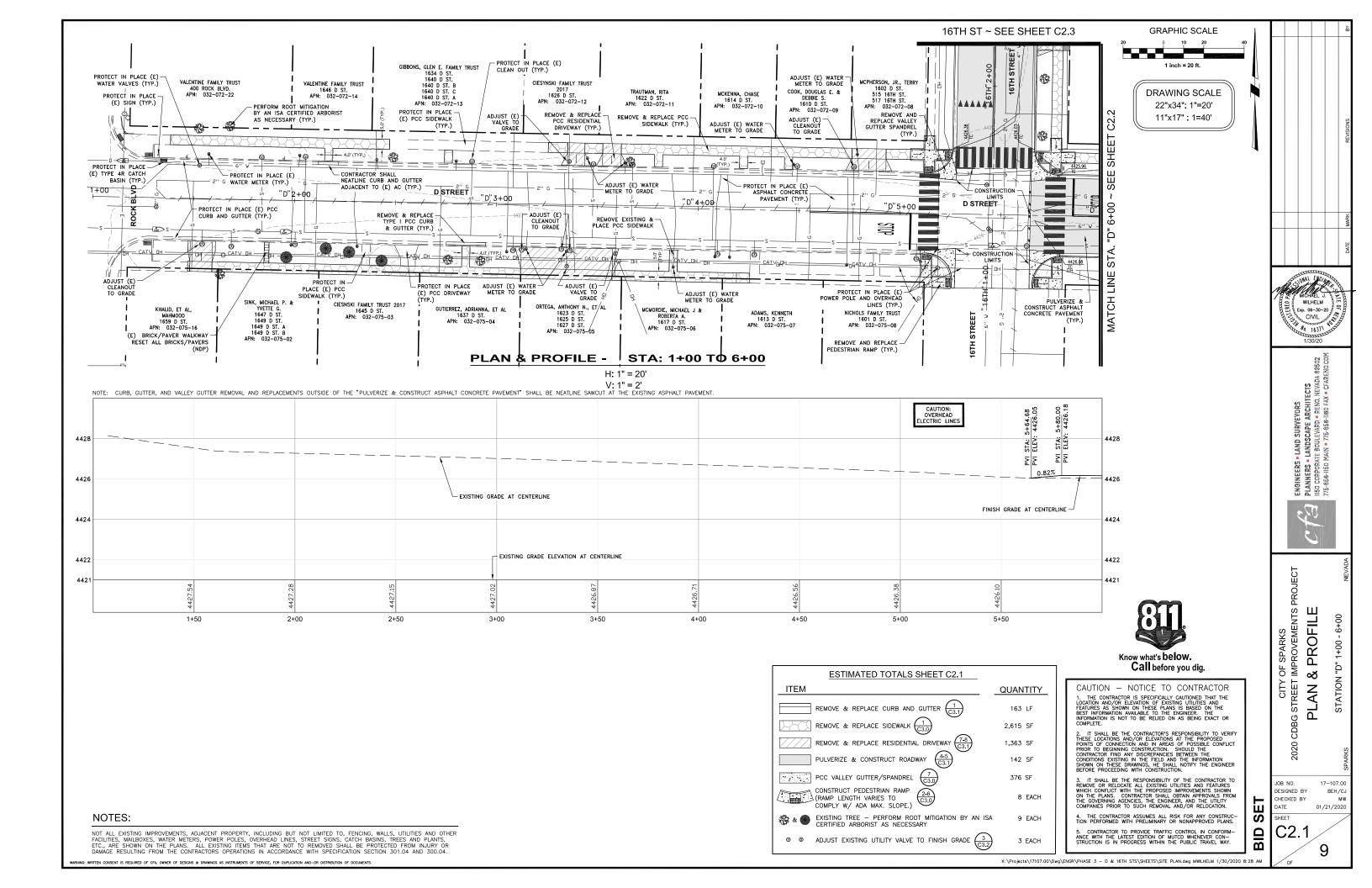
CAUTION - NOTICE TO CONTRACTOR 1. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AND FEATURES AS SHOWN ON THESE PLANS IS BASED ON THE BEST INFORMATION AVAILABLE TO THE ENGINEER. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE.

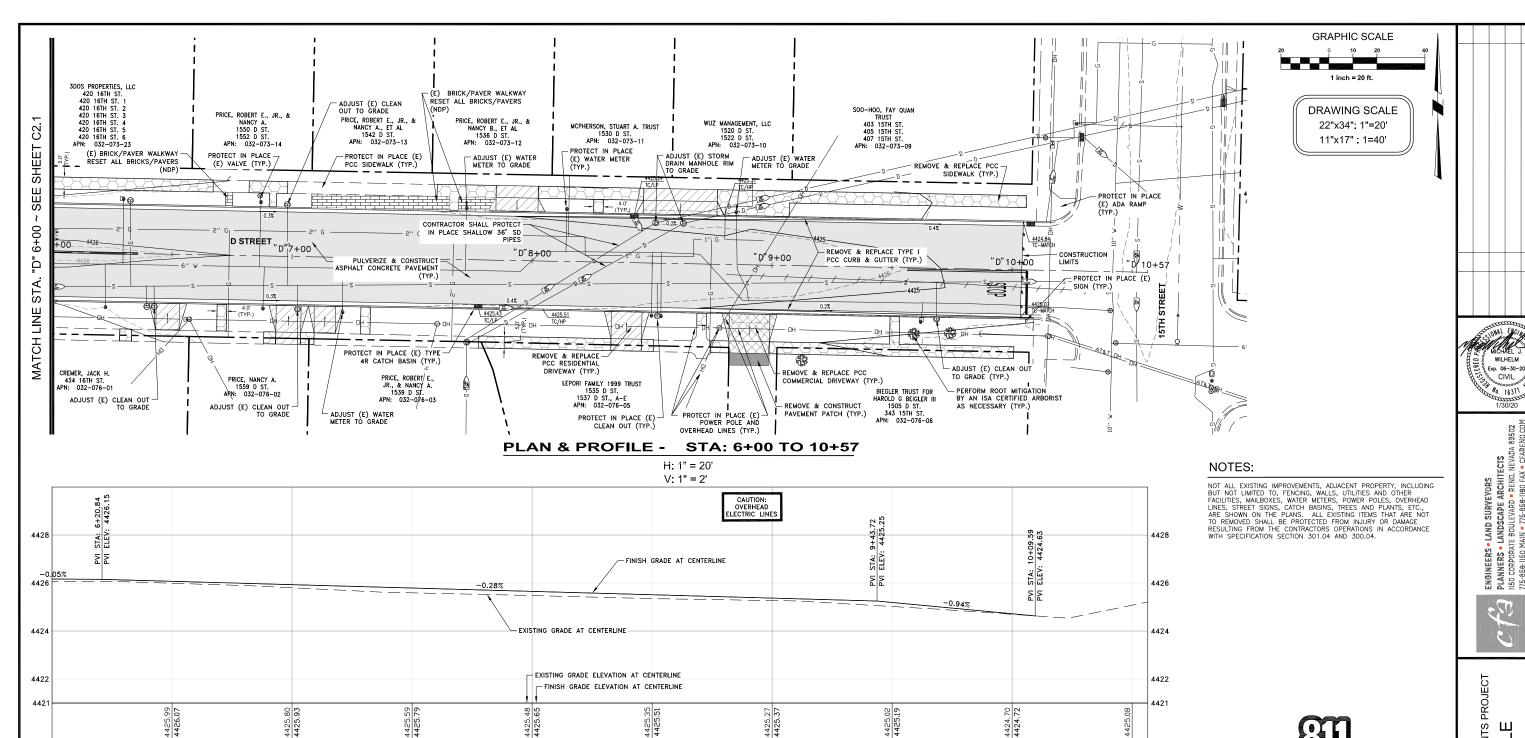
2. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THESE LOCATIONS AND/OR ELEVATIONS AT THE PROPOSED POINTS OF CONNECTION AND IN AREAS OF POSSIBLE CONFLICT PRIOR TO BEGINNING CONSTRUCTION. SHOULD THE CONTRACTOR FIND ANY DISCREPANCIES BETWEEN THE CONDITIONS EXISTING IN THE FIELD AND THE INFORMATION SHOWN ON THESE DRAWINGS, HE SHALL NOTIFY THE ENGINEER BEFORE PROCEEDING WITH CONSTRUCTION.

3. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO REMOVE OR RELOCATE ALL EXISTING UTILITIES AND FEATURES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS. CONTRACTOR SHALL OBTAIN APPROVALS FROM THE GOVERNING AGENCIES, THE ENGINEER, AND THE UTILITY COMPANIES PRIOR TO SUCH REMOVAL AND/OR RELOCATION.

4. THE CONTRACTOR ASSUMES ALL RISK FOR ANY CONSTRUCTION PERFORMED WITH PRELIMINARY OR NONAPPROVED PLANS.

5. CONTRACTOR TO PROVIDE TRAFFIC CONTROL IN CONFORMANCE WITH THE LATEST EDITION OF MUTCD WHENEVER CONSTRUCTION IS IN PROGRESS WITHIN THE PUBLIC TRAVEL WAY.





9+00

9+50

4425.35

8+50

BACK OF SIDEWALK

(RIGHT)

EXISTING | PROPOSED

4,425.42

4,425.17

4,425.00

4,425.43

4,425.24

4,425.34

4,424.95

4,424.83

Know what's below.

Call before you dig.

CAUTION - NOTICE TO CONTRACTOR 1. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AND FEATURES AS SHOWN ON THESE PLANS IS BASED ON THE BEST INFORMATION AVAILABLE TO THE ENGINEER. THE HYPERMATION IS NOT TO BE RELIED ON AS BEING EXACT OR

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5. CONTRACTOR TO PROVIDE TRAFFIC CONTROL IN CONFORM ANCE WITH THE LATEST EDITION OF MUTCD WHENEVER CONSTRUCTION IS IN PROGRESS WITHIN THE PUBLIC TRAVEL WAY.

ITEM	QUANTITY	
REMOVE & REPLACE CURB AND GUTTER (1)	808 LF	
REMOVE & REPLACE SIDEWALK (1)	2,704 SF	
REMOVE & REPLACE RESIDENTIAL DRIVEWAY $\frac{7-8}{C3.1}$	1,257 SF	
COMMERCIAL DRIVEWAY (7.8)	311 SF	
PULVERIZE & CONSTRUCT ROADWAY (4-5)	14,935 SF	
PAVEMENT PATCH 3 G3.1	85 SF	
EXISTING TREE — PERFORM ROOT MITIGATION BY AN IS CERTIFIED ARBORIST AS NECESSARY	A 3 EACH	

ADJUST EXISTING SEWER/STORM DRAIN MANHOLE (3.2)

10+00

ESTIMATED TOTALS SHEET C2.2

10+50

2 EACH

4. THE CONTRACTOR ASSUMES ALL RISK FOR ANY CONSTRUCTION PERFORMED WITH PRELIMINARY OR NONAPPROVED PLANS.

 \overline{S}

RNING; WRITTEN CONSENT IS REQUIRED OF CFA, OWNER OF DESIGNS & DRAWINGS AS INSTRUMENTS OF SERVICE, FOR DUPLICATION AND-OR DISTRIBUTION OF DOCUMEN

LEFT LIP

4,425.41

4,425.32

4,425.16

4,424.97

4,424.87

4.424.88

4,424.68

4,424.49

EXISTING | PROPOSED | OFFSET

4,425.39

4,425.25

4,425.12

4,424.98

4.424.87

4.424.86

4,424.66

4,424.47

18.5

18.5

18.5

18.5

18.4

18.4

18.4

4425.99

6+50

BACK OF SIDEWALK

(LEFT)

EXISTING PROPOSED

4,426.08

4,425.19

4,425.03

4,424.87

4,426.10

4,425.55

4,425.33

4.425.08

4,425.03

4,424.88

STATION

6+50

7+00

7+50

8+00

8+50

9+00

9+50

10+00

93 4425.8

7+00

7+50

DESIGN TABLE D STREET SHEET C2.4

4,425.99

4,425.80

4,425.59

4,425.48

4,425.35

4,425.27

4,425.02

4,424.70

CROWN

EXISTING | PROPOSED | OFFSET

4,426.07

4,425.93

4,425.79

4,425.65

4,425.51

4.425.37

4,425.19

4,424.72

18.5

18.5

18.5

18.5

18.5

18.5

18.5

18.5

8+00

4,425.49

4,425.34

4,425.19

4,425.13

4,424.96

4.424.89

4,424.74

4,424.72

RIGHT LIP

EXISTING PROPOSED

4,425.47

4,425.30

4,425.14

4,425.13

4.425.04

4.424.93

4,424.81

4,424.70

BID

CITY OF SPARKS STREET IMPROVEMENTS PROJECT ∞ర PLAN CDBG

PROFILE

. - 00+9

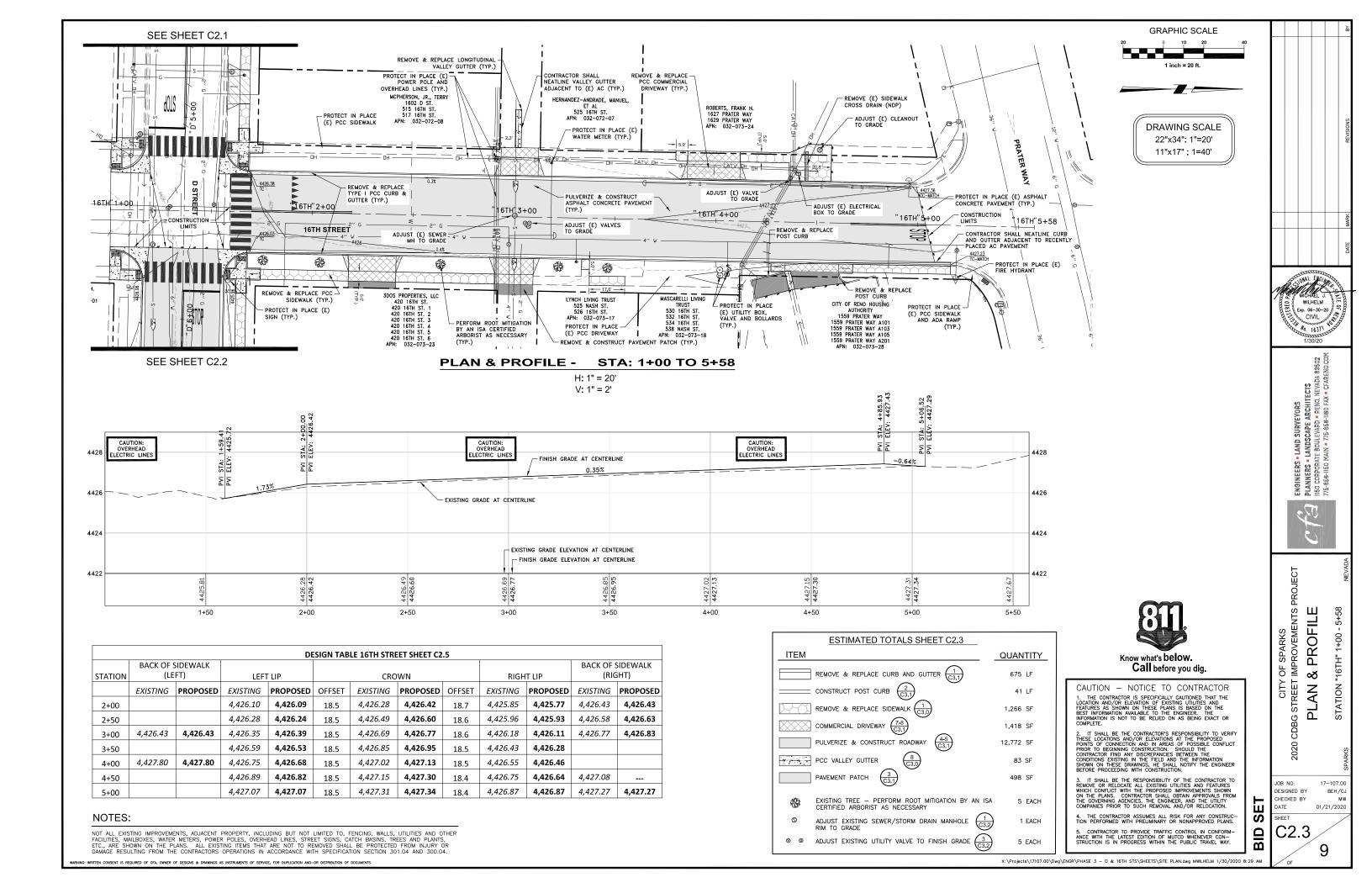
STATION

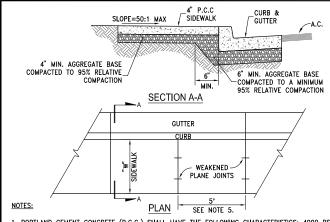
IOR NO. DESIGNED BY BEH/C

9

HECKED BY DATE 01/21/2020 SHEET

C2.2

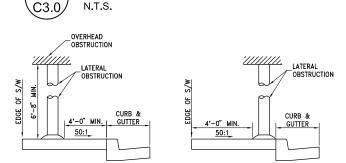




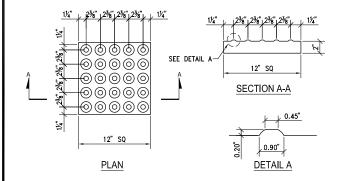
- . PORTLAND CEMENT CONCRETE (P.C.C.) SHALL HAVE THE FOLLOWING CHARACTERISTICS: 4000 PSI MIN. COMPRESSIVE STRENGTH AT 28 DAYS, MIN. 6 SACKS OF CEMENT PER CUBIC YARD WITH MAX. WATER-CEMENT RATIO OF 0.45. AIR ENTRAINMENT 6% ± 1.5%. SLUMP AT 1 TO 4 INCHES, MIX DESIGN SHALL CONFORM TO THE REQUIREMENTS OF SECTION 337 OF STANDARD SPECIFICATIONS OF PUBLIC WORKS CONSTRUCTION (SSPWC), AS ADOPTED BY CITY COUNCIL. CEMENT SHALL BE TYPE II. ALL CEMENT CONCRETE SHALL HAVE A COARSE AGGREGATE GRADATION CONFORMING TO SIZE No. 67. POLYPROPYLENE OR CELLULOSE FIBERS SHALL BE ADDED TO THE P.C.C. AT 1.5 LBS. PER CUBIC YARD. ALL MATERIALS SHALL CONFORM TO SSPWC, AS ADOPTED BY CITY COUNCIL.
- AGGREGATE BASE MATERIAL UNDER SIDEWALKS SHALL BE TYPE 2, CLASS B CRUSHED AGGREGATE BASE. MATERIALS SHALL CONFORM TO SSPWC SECTION 200, AS ADOPTED BY CITY COUNCIL.
- SIDEWALK WIDTH "W" SHALL BE 4 FT MIN. WITH 60" PASSING SPACE EVERY 200' ON RESIDENTIAL STREETS AND 6 FT MIN. ON COLLECTOR AND ARTERIAL STREETS.
- WEAKENED PLANE JOINTS SHALL BE CONSTRUCTED AT 5 FT INTERVALS AND ACCORDANCE WITH SECTION 312 OF THE SSPWC, AS ADOTED BY CITY COUNCIL.

SIDEWALK DETAIL

- 5. ALL ADJACENT CONCRETE REMOVAL SHALL BE TO NEAT SAW CUT LINES AT RIGHT ANGLES TO NEW SIDEWALK. DOWEL INTO EXISTING ADJACENT CONCRETE SIDEWALK WITH A MINIMUM OF TWO (2) No. REINFORCEMENT BARS EQUALLY SPACED ACROSS WIDTH "W". DOWELS SHALL PENETRATE A MINIMUM OF
- 6. SIDEWALKS SHALL NOT BE POURED MONOLITHICALLY WITH CURBS.
- 7. TUNNELING AND/OR BORING IS NOT ALLOWED.



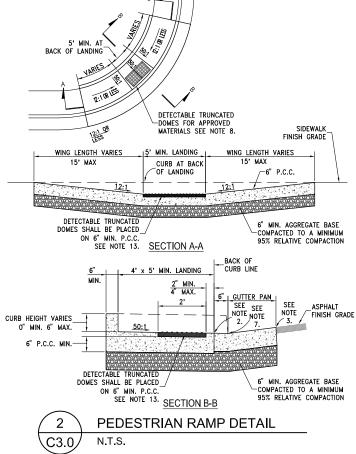
TYPICAL SIDEWALK vs OBSTRUCTION CLEARANCE DETAIL



NOTE: FOR DETECTABLE TRUNCATED DOMES APPROVED

TRUNCATED DOMES DETAIL

5 PEDESTRIAN RAMP DETAIL

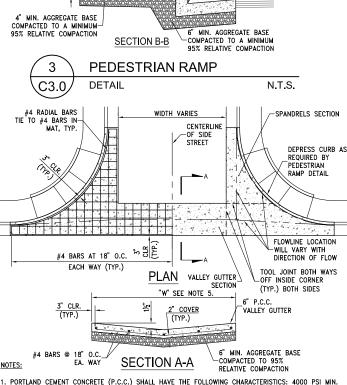


- STORM DRAIN INLETS OR SIMILAR ACCESSES SHALL NOT BE LOCATED IN THE AREA AT THE BASE
 OF THE CURB RAMP OR LANDING AREA. IF OBSTRUCTIONS SUCH AS INLETS, UTILITY POLES, PULL
 BOXES, FIRE HYDRANTS, ETC. ARE ENCOUNTERED, THE LOCATION AND DIMENSIONS MAY BE ADJUSTED UPON APPROVAL OF THE CITY ENGINEER.
- 2. NO LIP SHALL BE PERMITTED AT THE CURB RAMP SLOPE TO GUTTER PAN.
- 3. PLANTMIX BITUMINOUS SURFACE SHALL BE FLUSH WITH THE EDGE OF THE GUTTER PAN IN THE AREA OF THE CURB RAMP.
- 4. ROUGH BROOM TEXTURE ON CURB RAMPS AND WINGS.
- 5. DETECTABLE WARNING SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS AND SHALL BE PLACED ON MIN. SIX (6") INCHES OF P.C.C.
- 6. ALL SLOPE RATES ARE RELATIVE TO LEVEL AND SHALL COMPLY WITH THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) STANDARDS.
- 7. GUTTER SHALL MAINTAIN POSITIVE DRAINAGE TO PREVENT PONDING.
- 8. DETECTABLE WARNING SHALL CONSIST OF PRECAST WETSET TILES WITH MIN. SIZE OF 2' X 2' COLOR YELLOW. APPROVED PRODUCTS INCLUDE: "CASTINTACT 3". "TEKWAY DOME-TILES". AND "ACCESS TILE". DETECTABLE WARNING SURFACES SHALL EXTEND THE FULL WIDTH OF THE RAMP RUN (EXCLUDING ANY FLARED SIDES), BLENDED TRANSITION, OR TURNING SPACE, DETECTABLE WARNING SHALL BE CONSTRUCTED PER MANUFACTURER'S INSTALLATION GUIDELINES AND CONFORM
- 9. CONCRETE REMOVAL SHALL BE TO NEAT SAW CUT LINES
- 10. AGGREGATE BASE MATERIAL UNDER PEDESTRIAN RAMPS SHALL BE TYPE 2, CLASS B CRUSHED AGGREGATE BASE. MATERIALS SHALL CONFORM TO SSPWC SECTION 200, AS ADOPTED BY CITY
- 11. PORTLAND CEMENT CONCRETE (P.C.C.) SHALL HAVE THE FOLLOWING CHARACTERISTICS: 4000 PSI MIN. COMPRESSIVE STRENGTH AT 28 DAYS, MIN. 6 SACKS OF CEMENT PER CUBIC YARD WITH MAX. WATER-CEMENT RATIO OF 0.45, AIR ENTRAINBENT 6% ±1.5%, SLUMP AT 1 TO 4 INCHES. MIX DESIGN SHALL CONFORM TO THE REQUIREMENTS OF SECTION 337 OF STANDARD SPECIFICATIONS OF PUBLIC WORKS CONSTRUCTION (SSPWC), AS ADOPTED BY CITY COUNCIL. CEMENT SHALL BE TYPE II.

 ALL CEMENT CONCRETE SHALL HAVE A COARSE AGGREGATE GRADATION CONFORMING TO SIZE No.

 67. POLYPROPYLENE OR CELLULOSE FIBERS SHALL BE ADDED TO THE P.C.C. AT 1.5 LBS. PER CUBIC YARD. ALL MATERIALS SHALL CONFORM TO SSPWC, AS ADOPTED BY CITY COUNCIL
- 12. CONTRACTORS SHALL CORRECT ANY GRADE CONFLICT WITH EXISTING BOXES. THE CITY ENGINEER SHALL MAKE THE FINAL DETERMINATION REGARDING THE DEGREE OF MODIFICATIONS REQUIRED BY THE CONTRACTOR FOR GRADE CONFLICTS BETWEEN EXISTING BOXES AND NEW PEDESTRIAN RAMPS.
- 13. SIDEWALK AT BOTH SIDES OF RAMP MAY BE RECONSTRUCTED TO MINIMIZE THE GRADE AT A HORIZONTAL DISTANCE TO BE DETERMINED IN THE FIELD, UPON APPROVAL OF THE CITY ENGINEER, SUBJECT TO PROWAG REQUIREMENTS. CURB AT THE BACK OF WALK MAY BE NEEDED. A TRANSITION SECTION OF SIDEWALK MAY BE NECESSARY TO MATCH CROSS SLOPE OF EXISTING SIDEWALK TO PEDESTRIAN RAMP IMPROVEMENTS. TRANSITION SECTIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.





BACK OF SIDEWALK

\$0.₇

PLAN

12:1 OR LES

CURB & GUTTER

SEE NOTE 7

SIS.

0:1

6" P.C.C

6" MIN. AGGREGATE BASE

COMPACTED TO A MINIMUM-

95% RELATIVE COMPACTION

CONCRETE SIDEWALK
WIDTH VARIES

50:1

SECTION A-A

50:1 MAX CROSS SLOPE

4" MIN. AGGREGATE BASE

95% RELATIVE COMPACTION

COMPACTED TO A MINIMUM

ALL DIRECTIONS

CURB AND

DETECTABLE TRUNCATED
--DOMES FOR APPROVED

MATERIALS SEE NOTE 8.

FINISH GRADE

6" | GUTTER PAN |

SEE NOTE 2.

SEE NOTE 7.

SEE NOTE 5.

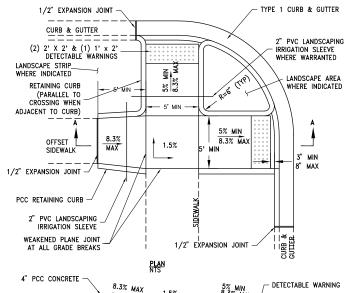
DETECTABLE TRUNCATED

CONCRETE DOMES SHALL BE PLACED ON 6" MIN. P.C.C.

1. PORTLAND CEMENT CONCRETE (P.C.C.) SHALL HAVE THE FOLLOWING CHARACTERISTICS: 4000 PSI MIN. COMPRESSIVE STRENGTH AT 28 DAYS, MIN. 6 SACKS OF CEMENT FER CUBIC YARD WITH MAX.
WATER-CEMENT RATIO OF 0.45, AIR ENTRAINMENT 6% ±1.5%, SLUMP AT 1 TO 4 INCHES. MIX DESIGN
SHALL CONFORM TO THE REQUIREMENTS OF SECTION 337 OF STANDARD SPECIFICATIONS OF PUBLIC WORKS CONSTRUCTION (SSPWC), AS ADDPTED BY CITY COUNCIL. CEMENT SHALL BE TYPE II. ALL CEMENT CONCRETE SHALL HAVE A COARSE AGGREGATE GRADATION CONFORMING TO SIZE No. 67. POLYPROPYLENE OR CELLULOSE FIBERS SHALL BE ADDED TO THE P.C.C. AT 1.5 LBS. PER CUBIC YARD. ALL MATERIALS SHALL CONFORM TO SSPWC, AS ADOPTED BY CITY COUNCIL

- 2. ACGREGATE BASE UNDER VALLEY GUTTER AND SPANDRELS SHALL BE TYPE 2, CLASS B CRUSHED AGGREGATE. ALL MATERIALS SHALL CONFORM TO SSPWC SECTION 200, AS ADOPTED BY CITY COUNCIL.
- 3. P.C.C VALLEY GUTTER DETAIL FOR RESIDENTIAL OR COMMERCIAL ZONES ONLY FOR OTHER APPLICATIONS AN ENGINEERED DESIGN IS REQUIRED.
- 4. VALLEY GUTTER SHALL HAVE WEAKENED PLANE JOINTS EVERY 10 FEET.
- 5. LOCAL AND COLLECTOR STREETS, "W"=6' MIN. ARTERIAL STREETS (DRIVEWAYS ONLY), "W"=10' MIN.
- 6. YALLEY GUTTER SECTIONS (SPANDRELS) ALONG CURB & GUTTER MAY BE A MONOLITHIC POUR AS SHOWN. DOWELS MATCHING REBAR SPACING SHOWN ARE REQUIRED FROM VALLEY GUTTER SECTION TO SPANDREL SECTION IF POURED SEPARATELY.

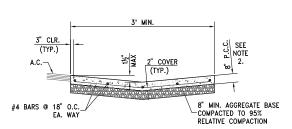
PCC VALLEY GUTTER C3.0 N.T.S.



5% MIN 8.3% MAX PCC RETAINING CURB MAINTAIN SIDEWALK GRADE PCC RETAINING CURE 4" TYPE 2, CLASS B AGGREGATE BASE - MAINTAIN SIDEWALK OR LANDING GRADE COMPACTED TO 95% 2" PVC LANDSCAPI SECTION A-A IRRIGATION SLEEVE -18" BELOW FINISH GRADE NOTES: RETAINING CURBS ARE TO BE USED IN ALL CASES AND FINISH GRADE AT BACK OF

PEDESTRIAN RAMP DIRECTIONAL DETAIL C3.0

SIDEWALK IS TO BE RAISED TO TOP OF RETAINING CURB ELEVATION.



- 1. THIS GUTTER DESIGN IS FOR USE ON CITY STREETS UPON APPROVAL OF THE CITY ENGINEER.
- 2. PORTLAND CEMENT CONCRETE (P.C.C.) SHALL HAVE THE FOLLOWING CHARACTERISTICS: 4000 PSI MIN. COMPRESSIVE STRENGTH AT 28 DAYS, MIN. 6 SACKS OF CEMENT PER CUBIC YARD WITH MAX. WATER-CEMENT RATIO OF 0.45, AIR ENTRAINMENT 6% ±1.5%, SLUMP AT 1 TO 4 INCHES. MIX DESIGN SHALL CONFORM TO THE REQUIREMENTS OF SECTION 337 OF STANDARD SPECIFICATIONS OF PUBLIC WORKS CONSTRUCTION (SSPWC), AS ADOPTED BY CITY COUNCIL CEMENT SHALL BE TYPE II. ALL CEMENT CONCRETE SHALL HAVE A COARSE AGGREGATE GRADATION CONFORMING TO SIZE No. 67. POLYPROPYLENE OR CELLULOSE FIBERS SHALL BE ADDED TO THE P.C.C. AT 1.5 LBS. PER CUBIC YARD. ALL MATERIALS SHALL CONFORM TO SSPWC, AS ADOPTED BY CITY COUNCIL.
- AGGREGATE BASE MATERIAL UNDER VALLEY GUTTERS SHALL BE TYPE 2, CLASS B CRUSHED AGGREGATE BASE. MATERIALS SHALL CONFORM TO SSPWC SECTION 200, AS ADOPTED BY CITY
- WEAKENED PLANE JOINTS SHALL BE CONSTRUCTED AT 10 FT INTERVALS AND ACCORDANCE WITH SECTION 312 OF THE SSPWC, AS ADOPTED BY CITY COUNCIL.



Know what's below. Call before you dig.

LONGITUDINAL VALLEY GUTTER C3.0

S $\mathbf{\omega}$

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X:\Projects\17107.00\Dwg\ENGR\PHASE 3 - D & 16TH STS\SHEETS\C3.0-DETAILS.dwg MWILHELM 1/30/2020 8:30 AM

CFA, LAND CIVIL LAND 1150 C

WILHELM

Exp. 06-30-2

Na 16371

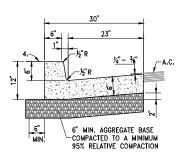
ETAIL

CITY OF SPARKS STREET IMPROVEMENTS $\overline{\Box}$ CIVIL CDBG

BEH/C

DESIGNED BY HECKED B' DATE 01/21/2020 SHEET

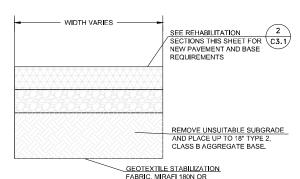
NOTES:



TYPE 1A

- 1. PORTLAND CEMENT CONCRETE (P.C.C.) SHALL HAVE THE FOLLOWING CHARACTERISTICS: 4000 PSI MIN. COMPRESSIVE STRENGTH AT 28 DAYS, MIN. 6 SACKS OF CEMENT PER CUBIC YARD WITH MAX. WATER-CEMENT RATIO OF 0.45, AIR ENTRAINMENT 6% $\pm 1.5\%$, SLUMP AT 1 TO 4 INCHES. MIX DESIGN SHALL CONFORM TO THE REQUIREMENTS OF SECTION 337 OF STANDARD SPECIFICATIONS OF PUBLIC
 WORKS CONSTRUCTION (SSPWC), AS ADOPTED BY CITY COUNCIL CEMENT SHALL BE TYPE II. ALL CEMENT
 CONCRETE SHALL HAVE A COARSE AGGREGATE GRADATION CONFORMING TO SIZE No. 67. POLYPROPYLENE OR CELLULOSE FIBERS SHALL BE ADDED TO THE P.C.C. AT 1.5 LBS. PER CUBIC YARD. ALL MATERIALS SHALL CONFORM TO SSPWC, AS ADOPTED BY CITY COUNCIL.
- 2. AGGREGATE BASE MATERIAL UNDER AND BEHIND CURB AND GUTTER SHALL BE TYPE 2. CLASS B CRUSHED AGGREGATE BASE. MATERIALS SHALL CONFORM TO SSPWC SECTION 200, AS ADOPTED BY CITY
- 3. WEAKENED PLANE JOINTS SHALL BE EVERY 10 FEET AND LOCATED ON THE BACK, TOP AND FACE OF THE CURB AND THE TOP OF THE GUTTER PAN.
- 4 CURR & GUTTER SECTIONS SHALL BE PLACED SEPARATELY FROM SIDEWALK SECTIONS. WHEN SIDEWALK IS NOT REQUIRED DIRECTLY BEHIND THE CURB, BACKFILL TO TOP OF CURB FOR A HORIZONTAL DISTANCE OF 12" FROM BACK FACE OF CURB AND COMPACT TO 90% RELATIVE COMPACTION.
- 5. FOR REPLACEMENT OF EXISTING CURB AND GUTTER, MATCH EXISTING TYPE.





APPROVED EQUAL. TYPICAL STABILIZATION SECTION

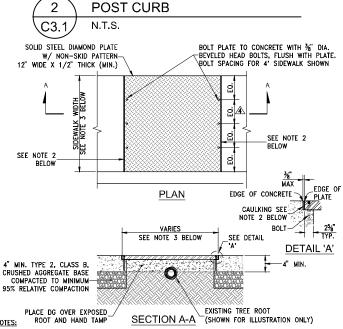
 $\underline{\mathsf{NOTE}}.$ LIMITS OF OVEREXCAVATION, WIDTHS, AND DEPTH TO BE DETERMINED BY THE ENGINEER.

PCC SURFACE PORTLAND CEMENT CONCRETE 6" MIN. TYPE 2. CLASS B SUBGRADE TO BE COMPACTED TO 90% M.D.D.

1. PORTLAND CEMENT CONCRETE (P.C.C.) SHALL HAVE THE FOLLOWING CHARACTERISTICS: 4000 PSI MIN. COMPRESSIVE STRENGFI AT 28 DATS, MIN. 6 SACKS OF CEMENT PER CUBIC YARD WITH MAX. WATER-CEMENT RATIO OF 0.45, AIR ENTERMINENT ST. ± 1.5%, SLUMP AT 1 TO 4 NICHES. MIX DESIGN SHALL CONFORM TO HE REQUIREMENTS OF SECTION 337 OF STANDARD SPECIFICATIONS OF PUBLIC WORKS CONSTRUCTION (SSPWC), AS ADOPTED BY CITY COUNCIL CEMENT SHALL BE TYPE II. ALL CEMENT CONCRETE SHALL HAVE A COARSE ACCRECATE GRADATION CONFORMING TO SIZE NO. 67, POLYPROPILEME OR CELLULOSE TIBERS SHALL HAVE AND ALL MANERAL SHALL CONFORM TO SSPWC, AS ADOPTED BY CITY OF THE P.C.C. AT 1.5 LES. PER CUBIC YARD ALL MANERALS SHALL CONFORM TO SSPWC, AS ADOPTED BY CITY OF THE P.C.C. AT 1.5 LES. PER CUBIC YARD ALL MANERALS SHALL CONFORM TO SSPWC, AS ADOPTED BY CITY OF THE P.C.C. AT 1.5 LES. PER CUBIC YARD ALL MANERALS SHALL CONFORM TO SSPWC, AS ADOPTED BY CITY OF THE P.C.C. AT 1.5 LES. PER CUBIC YARD. ALL MANERALS SHALL CONFORM TO SSPWC, AS ADOPTED BY CITY OF THE P.C.C. THE P.C. THE P

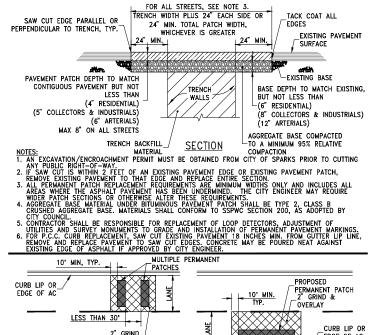
2. EXPANSION JOINTS 1/2-INCH WIDE SHALL BE LOCATED IN CURBS AND GUTTERS AT EACH SIDE OF STRUCTURES, AT THE EMBS OF ALL CURB RETURNS, AND ABUTTING HARDENED IN-PLACE CURB AND GUTTER, EXCEPT THAT EXPANSION JOINTS SHALL NOT BE INSTALLED WITHIN 20 FEET OF AN ISLAND NOSE. EXPANSION JOINTS SHALL BE 1/2-INCH THICK, SHAPED TO THE CROSS SECTION OF THE CURB AND GUTTER, AND CONSTRUCTED AT RICHT ANGES TO THE CURB AND GUTTER. AND CONSTRUCTED AT RICHT ANGES TO THE CURB AND GUTTER. JOINTS SHALL BE EVERT TO FEET AND LOCATED ON THE BACK, TOP AND EDGE OF THE CURB.

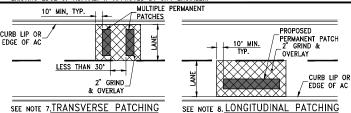
3. CURB AND GUTTER SECTIONS SHALL BE PLACED SEPARATELY FROM SIDEWALK SECTIONS



- 1. CONTRACTOR SHALL REMOVE ONLY THE EXISTING CONCRETE WITHIN THE ROOT MITIGATION LIMITS. THE CONTRACTOR SHALL EMPLOY AN ISA CERTIFIED ARBORIST TO PERFORM ROOT CROWN EXCAVATION / ROOT PRUNING. ISA CERTIFIED ARBORIST SHALL COORDINATE THIS WORK WITH THE CITY OF SPARKS URBAN FORESTER (334-2270).
- 2. SET PLATE FLUSH WITH ADJACENT CONCRETE AND PROVIDE 3" MAX GAP BETWEEN STEEL PLATE AND EDGE OF SIDEWALK, FILL GAP WITH CAULKING AFTER PLACEMENT OF PLATE. CAULKING MATERIAL SHALL BE GE SILICONE II* W/D SUPREME, OR APPROVED EQUAL. COLOR SHALL BE WHITE.
- 3. ROOT PLATE WIDTH SHALL MATCH WIDTH OF SIDEWALK. ROOT PLATE LENGTH VARIES, FINAL LENGTH SHALL BE DETERMINED IN THE FIELD BY THE CERTIFIED ARBORIST AND ENGINEER.
- △ BOLTS: ¾" DIA x 2 ¾" LONG DYNABOLT SLEEVE ANCHOR RAMSET REDHEAD FS-3826 OR APPROVED EQUAL. SIDEWALK WIDER THAN 4FT, BOLTS SHALL BE PLACED AT 18" O.C. FOR WIDTH OF SIDEWALK.
- 5. CONTRACTOR SHALL PROTECT TREES AT ALL TIMES, INCLUDING BUT NOT LIMITED TO TRUNKS, LIMBS AND ROOTS. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COSTS ASSOCIATED WITH REPAIR OR REMOVAL OF DAMAGED TREES DUE TO CONSTRUCTION ACTIVITIES.



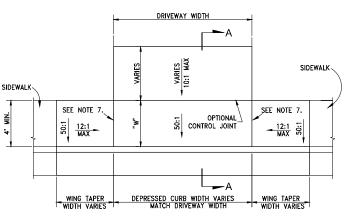


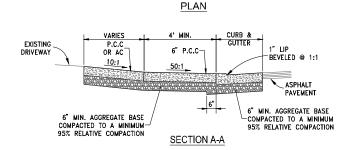


ALL STREETS WITH PAVEMENT CONDITION INDEX (PCI) GREATER THAN 65

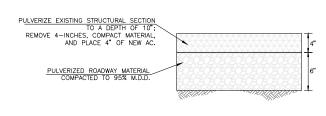
- 7. TRANSVERSE PATCHES SHALL INCLUDE A GRIND AND OVERLAY WHEREVER THERE ARE MULTIPLE PATCHES WITHIN 30 FEET OF EACH OTHER. 2" MINIMUM FOR RESIDENTIAL, COLLECTORS & INDUSTRIALS. 3" MINIMUM FOR ARTERIALS.
- 8. LONGITUDINAL PATCHES SHALL INCLUDE GRIND AND OVERLAY TO THE LANE LINES (BIKE, PARKING, OR TRAVEL). 2" MINIMUM FOR RESIDENTIAL, COLLECTORS & INDUSTRIALS. 3" MINIMUM FOR ARTERIALS.
- 9. DRIVEWAY & INTERSECTION LIMITS TO BE DETERMINED BY STREET CUT COORDINATOR.







P.C.C. DRIVEWAY APRON C3.1 DETAIL N.T.S.



STRUCTURAL PAVEMENT SECTION NOTES:

PLANTMIX BITUMINOUS PAVEMENT SHALL BE A TYPE 3, 50 BLOW, 3% AIR VOID MIX USING PG64—28NV ASPHALT CEMENT. AGGREGATES SHALL BE PRE—MARINATED WITH LIME PER NDOT SPECIFICATIONS. RECYCLED ASPHALT PAVEMENT (RAP) UP TO 15% WILL BE ALLOWED; HOWEVER, ALL VIRGIN AGGREGATES WILL BE SUBJECT TO LIME PRE—MARINATING PER NDOT SPECIFICATIONS.

GEOTECHINCAL REPORT NOT PROVIDED. ROADWAY ASPHALT SECTION THICKNESS, AGGREGATE BASE, AND SUBGRADE PREPARATION HAS BEEN PROVIDED BY THE CITY OF SPARKS FOR ROADWAY IMPROVEMENTS. NO WARRANTIES OR GUARANTEES ARE IMPLIED OR PROVIDED FOR THE MATERIALS PERFORMANCE OF ROADWAY AREA. PLANS PROVIDE LIMITS OF ASPHALT, CURB AND GUTTER, AND REMOVAL OF PROPOSED

OVEREXCAVATE AND STABILIZE ANY UNSUITABLE SUBGRADE MATERIAL AS DIRECTED BY THE ENGINEER PRIOR TO PAVING OPERATIONS. REFERENCE DETAIL 5, SHEET C3.1.



ASPHALT SECTION DETAIL

16TH ST. & D ST.

N.T.S.

NOTES:

- 1. PORTLAND CEMENT CONCRETE (P.C.C.) SHALL HAVE THE FOLLOWING CHARACTERISTICS: 4000 PSI MIN. COMPRESSIVE STRENGTH AT 28 DAYS, MIN. 6 SACKS OF CEMENT PER CUBIC YARD WITH MAX. WATER-CEMENT RATIO OF 0.45, AIR ENTRAINMENT 6% ±1.5%, SLUMP AT 1 TO 4 INCHES. MIX DESIGN SHALL CONFORM TO THE REQUIREMENTS OF SECTION 337 OF STANDARD SPECIFICATIONS OF PUBLIC WORKS CONSTRUCTION (SPWC), AS ADOPTED BY CITY COUNCIL CEMENT SHALL BE TYPE II.
 ALL CEMENT CONCRETE SHALL HAVE A COARSE AGGREGATE GRADATION CONFORMING TO SIZE No. 67. POLYPROPYLENE OR CELLULOSE FIBERS SHALL BE ADDED TO THE P.C.C. AT 1.5 LBS. PER
- AGGREGATE BASE MATERIAL UNDER DRIVEWAYS AND SIDEWALKS SHALL BE TYPE 2, CLASS B CRUSHED AGGREGATE BASE. ALL MATERIALS SHALL CONFORM TO SSPWC SECTION 200, AS ADOPTED BY CITY COUNCIL.
- 3. RESIDENTIAL DRIVEWAYS SHALL BE POURED SEPARATE FROM CURB AND GUTTER.
- . COMMERCIAL DRIVEWAYS MAY BE POURED MONOLITHIC WITH CURB AND GUTTER. COMMERCIAL DRIVEWAYS TO HAVE #4 BARS AT 18" ON CENTER LONGITUDINAL & TRANSVERSE EXTENDING INTO GUTTER PAN AND DRIVEWAY WINGS. MINIMUM 2" CONCRETE COVER FOR ALL REINFORCING BARS. WHEN COMMERCIAL DRIVEWAY APPROACH AND CURB & GUTTER IS POURED SEPARATELY, IT SHALL BE REQUIRED FOR EACH REINFORCING BAR TO BE DOWELED INTO ADJACENT CURB & GUTTER. DOWELS SHALL #4 REBAR, PENETRATE INTO CURB & GUTTER MINIMUM OF 6", SPACED AT 18" ON CENTER AND BE SECURELY TIED TO THE DRIVEWAY APPROACH REINFORCING
- 5. IF JOINT EXISTS WITHIN 4 FEET OF DRIVEWAY, REMOVE SIDEWALK AND CURB AND GUTTER TO THAT
- 6. ALL ADJACENT CONCRETE REMOVAL SHALL BE TO NEAT SAW CUT LINES AT RIGHT ANGLES. DOWEL INTO EXISTING ADJACENT CONCRETE DRIVEWAY APPROACH OR SIDEWALK WITH (2) No. 4 REINFORCEMENT BARS EQUALLY SPACED ACROSS WIDTH "W". DOWELS SHALL PENETRATE A MINIMUM OF 4" INTO EXISTING CONCRETE.
- WEAKENED PLANE JOINTS SHALL BE CONSTRUCTED AT 5 FT INTERVALS AND IN ACCORDANCE WITH SECTION 312 OF THE SSPWC, AS ADOPTED BY CITY COUNCIL.
- 8. DETAIL S-111A SHALL NOT BE USED FOR NEW CONSTRUCTION UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER.



P.C.C. DRIVEWAY APRON NOTES C3.1

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SHEET C3.

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ING; WRITTEN CONSENT IS REQUIRED OF CFA, OWNER OF DESIGNS & DRAWINGS AS INSTRUMENTS OF SERVICE, FOR DUPLICATION AND-OR DISTRIBUTION OF DOCUMENTS

OVEREXCAVATION SECTION

FOR UNSUITABLE MATERIAL

N.T.S.

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NULS:

I. PORTLAND CEMENT CONCRETE (P.C.C.) SHALL HAVE THE FOLLOWING CHARACTERISTICS: 4000 PSI MIN. COMPRESSIVE STRENGTH AT 28 DAYS, MIN. 6 SACKS OF CEMENT PER CUBIC YARD WITH MAX. WATER-CEMENT RATIO OF 0.45, MR ENTRAINMENT 6% ±1.5%, SLUMP AT 1 TO 4 INCES, MIX DESIGN SHALL CONFORM TO THE REQUIREMENTS OF SECTION 337 OF STANDARD SPECIFICATIONS OF PUBLIC WORKS CONSTRUCTION (SSPWC), AS ADOPTED BY CITY COUNCIL. CEMENT SHALL BE TYPE II. ALL CEMENT CONCRETE SHALL HAVE A COARSE AGGREGATE GRADATION CONFORMING TO SIZE NO. 67. POLYPROPYLENE OR CELLULOSE FIBERS SHALL BE ADDED TO THE P.C.C. AT 1.5 LBS. PER CUBIC YARD. ALL MATERIALS SHALL CONFORM TO SSPWC, AS ADOPTED BY CITY COUNCIL.

CIRCUMSTANCES MAY REQUIRE THE NEED FOR SPECIAL TYPES OF TOP OF MANHOLE CONFIGURATIONS SUCH AS FLAT TOP, ABOVE GROUND, ETC. AS DIRECTED BY THE CITY OF SPARKS. DETAILED PLANS OF ANY SPECIAL TOP OF MANHOLE CONFIGURATIONS AND ASSOCIATED COLLARS MUST BE APPROVED BY

3. IN UNPAYED AREAS, IT SHALL BE NECESSARY TO SET THE MANHOLE RIM APPROXIMATELY 6 INCHES ABOVE THE SURROUNDING AREA. INSTALL A 6 INCH THICK RING OF CONCRETE, TAPERED AT A 3:1 SLOPE, FROM THE TOP, OUTSIDE EDGE OF THE COLLAR TO THE EXISTING GROUND SURFACE.

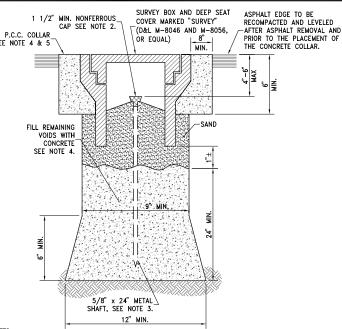
. EXISTING SANITARY SEWER MANHOLE LIDS LOCATED IN GUTTER PANS, SHALL HAVE NEW WATER TIGHT

ALL GRADE RING JOINTS ARE TO BE GROUTED WITH NON-SHRINK GROUT HAVING THE FOLLOWING CHARACTERISTICS: 3000 PSI MIN. COMPRESSIVE STRENGTH AT 28 DAYS, MIN. 6 SACKS OF CEMENT PER CUBIC YARD AND SLUMP AT 1 TO 4 INCHES. ALL MATERIAL SHALL CONFORM TO SSPWC, AS ADOPTED BY

ALL GRADE RINGS SHALL BE PORTLAND CEMENT CONCRETE. PVC GRADE RINGS ARE NOT ALLOWED. CONTRACTOR SHALL USE AS FEW GRADE RINGS AS POSSIBLE TO REACH REQUIRED DEPTH. HOWEVER NO CASE SHALL THE QUANTITY EXCEED 4 GRADE RINGS WITHOUT APPROVAL OF THE CITY ENGINEER.

. P.C.C. COLLARS IN ALL ROADWAYS SHALL BE PROTECTED FROM TRAFFIC LOADS UNTIL MINIMUM 3000 PSI IS

MANHOLE COLLAR C3.2 N.T.S.



NOTES:

1. FERROUS METAL OVER MONUMENT FOR RECOVERY BY DIP NEEDLE OR MAGNETIC INSERT IN CAP.

1. TOTAL THE NEEDLE OF MAGNETIC INSERT IN CAP.

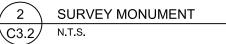
1. TOTAL THE NEEDLE OF MAGNETIC INSERT IN CAP.

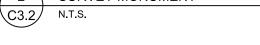
2. 1 1/2" MIN. NONFERROUS CAP WITH PROFESSIONAL LAND SURVEYOR NO. PERMANENTLY ATTACHED PRIOR TO PLACEMENT. PRE-PUNCHED CAPS SHALL NOT BE PERMITTED

3. 5/8" METALLIC SHAFT (SMOOTH SHAFTS TO BE DEFORMED).

 PORTLAND CEMENT CONCRETE (P.C.C.) SHALL HAVE THE FOLLOWING CHARACTERISTICS: 4000 PSI MIN. COMPRESSIVE STRENGTH AT 28 DAYS, MIN. 6 SACKS OF CEMENT PER CUBIC YARD WITH MAX. WATER—CEMENT RATIO OF 0.45, AIR ENTRAINMENT 6% ±1.5%, SLUMP AT 1 TO 4 INCHES. MIX DESIGN SHALL CONFORM TO THE REQUIREMENTS OF SECTION 337 OF STANDARD SPECIFICATIONS OF PUBLIC WORKS CONSTRUCTION (SSPWC), AS ADOPTED BY CITY COUNCIL. CEMENT SHALL BE TYPE II. ALL CEMENT CONCRETE SHALL HAVE A COARSE AGGREGATE GRADATION CONFORMING TO SIZE No. 67. POLYPROPYLENE OR CELLULOSE FIBERS SHALL BE ADDED TO THE P.C.C. AT 1.5 LBS. PER CUBIC YARD. ALL MATERIALS SHALL CONFORM TO SSPWC, AS ADOPTED BY CITY COUNCIL. 5. P.C.C. COLLARS IN ALL ROADWAYS SHALL BE PROTECTED FROM TRAFFIC LOADS UNTIL MINIMUM 3000 PSI IS

ATTAINED.







4" SPACING BETWEEN EDGES OF SOLID STRIPES -

NOTES

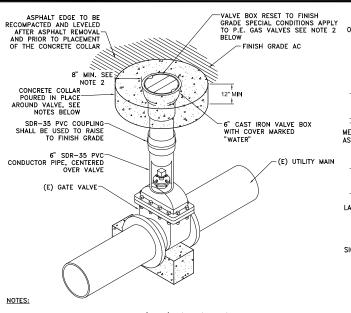
1 REFERENCE THE MANUAL OF UNIFORM TRAFFIC DEVICES (MUTCD) 2009 EDITION FOR STRIPING AND SIGNAGE INSTALLATION.

2. REFERENCE SHEET C1.1 FOR GENERAL NOTES.

3. REMOVE ALL EXISTING STRIPING AND PAVEMENT MARKINGS WHICH CONFLICT WITH PROPOSED MARKINGS BY GRINDING.

"STOP" PAVEMENT MARKING DETAIL





1. PORTLAND CEMENT CONCRETE (P.C.C.) SHALL HAVE THE FOLLOWING CHARACTERISTICS: 4000 PSI MIN. COMPRESSIVE STRENGTH AT 28 DAYS, MIN. 6 SACKS OF CEMENT PER CUBIC YARD WITH MAX. WATER-CEMENT RATIO OF 0.45, AIR ENTRAINNENT 6% ± 1.5%, SLUMP AT 1 TO 4 INCHES. MIX DESIGN SHALL CONFORM TO THE REQUIREMENTS OF SECTION 337 OF STANDARD SPECIFICATIONS OF PUBLIC NORKS CONSTRUCTION (SSPWC), AS ADOPTED BY CITY COUNCIL. CEMENT SHALL BE TYPE II. ALL CEMENT CONCRETE SHALL HAVE A COARSE AGGREGATE GRADATION CONFORMING TO SIZE NO. 67. POLYPROPYLENE OR CELLULOSE FIBERS SHALL BE ADDED TO THE P.C.C. AT 1.5 LBS. PER CUBIC YARD. ALL MATERIALS SHALL CONFORM TO SSPWC, AS ADOPTED BY CITY COUNCIL.

SPECIAL CONDITIONS APPLY TO P.E. GAS VALVES WITH TELESCOPING RISERS. CONTRACTORS SHALL NOT RAISE TO GRADE RISERS THAT HAVE BEEN CUT-OF DURING LOWERING. RISERS CUT-OFF DURING LOWERING SHALL BE REMOVED COMPLETELY TO MAIN AND TAKEN TO NY ENERGY FOR EXACT MATERIAL REPLACEMENT PRIOR TO RAISING TO GRADE.

FOR MULTIPLE VALVE/RISER BOXES IN CLOSE PROXIMITY, A MONOLITHIC CONCRETE

3 VALVE BOX COLLAR C3.2 N.T.S.

CENTER CROSSWALK CURB & GUTTER _(TYP.) LANDING (TYP.) 5' TYP. MEDIAN ISLAND AS APPLICABLE 2' WIDE BARS TO BE CENTERED BETWEEN LANE LINES AND ON LANE LINES LANE LINES-STOP BAR USED FOR STOP AND PLACE FROM EDGE OF SIGNALIZED CONDITION LANE TO EDGE OF OTHERWISE USE PAVEMENT OR LIP OF CURB. SEE NOTE 4. 12" | 24" TYPICAL CROSSWALK STRIPING DETAIL TRIANGLE HEIGHT IS EQUAL DIRECTION TO 1.5 TIMES THE BASE-OF TRAVEL DIMENSION YIELD BAR MARKINGS IOLES: ALL STRIPING SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) FOR STREETS AND HIGHWAYS, LATEST EDITION.

2. PREFORMED THERMOPLASTIC PAVEMENT MARKINGS SHALL BE INSTALLED PER MANUFACTURERS RECOMMENDATIONS AND IN ACCORDANCE WITH SECTION 634 OF THE LATEST EDITION OF "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" PUBLISHED BY THE STATE OF NEVADA, DEPARTMENT OF TRANSPORTATION (NDOT), EXCEPT ALL THERMOPLASTIC MARKINGS WILL BE 0.090 INCHES THICK, EXCEPT MARKING FOR BIKE LANES WHICH WILL BE 0.075 INCHES. HOT—APPLIED EXTRUDED THERMOPLASTIC SHALL NOT BE USED.

3. CROSSWALK MARKINGS SHALL BE INSTALLED AT ALL LOCATIONS AS DIRECTED BY ENGINEER.

C3.2

CROSSWALK LEGEND DETAIL N.T.S.

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CITY OF SPARKS STREET IMPROVEMENTS PROJECT DETAIL CIVIL CDBG

BEH/C 01/21/2020

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DESIGNED BY HECKED BY SET DATE SHEET C3.2

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Know what's below. Call before you dig.

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