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 1301361 - ALLEY WAY "3"

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 1301361 - ALLEY WAY "3"

CITY OF SPARKS

RONALD E. SMITH
DONALD ABBOTT
ED LAWSON
PAUL ANDERSON
CHARLENE BYBEE
KRISTOPHER DAHIR
NEIL KRUTZ

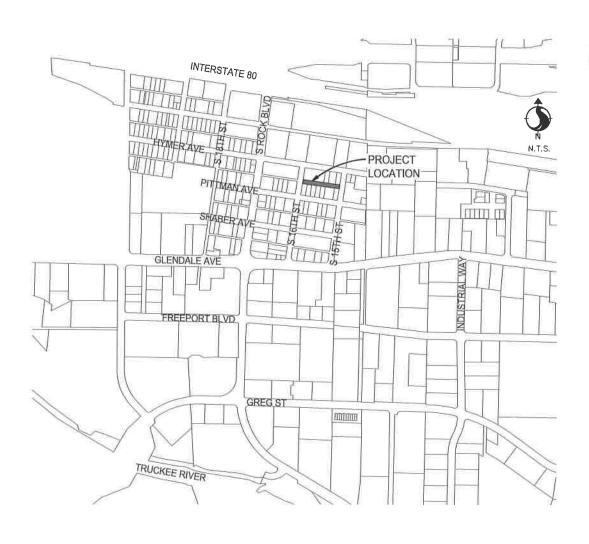
MAYOR

COUNCIL MEMBER WARD 1
COUNCIL MEMBER WARD 2

COUNCIL MEMBER WARD 3

COUNCIL MEMBER WARD 4
COUNCIL MEMBER WARD 5

CITY MANAGER



APPROVALS:



PLANS PREPARED AND SUBMITTED BY:



CLINT J. ALVERSON, P.E. DATE
PROJECT MANAGER

ENGINEER:







2. TOPOGRAPHIC INFORMATION CONTAINED WITHIN THESE CONSTRUCTION DOCUMENTS WAS PREPARED BY CONVENTIONAL FIELD TOPOGRAPHIC SURVEYS.

THE BASIS OF BEARING IS GRID NORTH, NAD 83 (2011) NEVADA STATE PLANE COORDINATE SYSTEM, WEST ZONE, UTILIZING A COMBINED GRID TO GROUND FACTOR OF 1.000197939

THE BASIS OF ELEVATION IS NAVD '88, BASED UPON CITY OF SPARKS BENCH MARK No. 59 ELEVATION = 4422.01

- 3. WORK IN PUBLIC STREETS, ONCE BEGUN, SHALL BE EXECUTED TO COMPLETION WITHOUT DELAY SO AS TO PROVIDE MINIMUM INCONVENIENCE TO ADJACENT PROPERTY OWNERS AND TO THE TRAVELING PUBLIC. THE CONSTRUCTION OF THE STREET IMPROVEMENTS SHALL ALLOW FOR THE PERPETUATION OF ALL EXISTING LEGAL ACCESSES AND EXISTING DRIVEWAYS, UNLESS OTHERWISE NOTED.
- 4. THE CONTRACTOR SHALL COOPERATE WITH OTHER CONTRACTORS OR UTILITY COMPANY FORCES WORKING ON THE SITE, AND WITH BUSINESS OWNERS ACTIVE OPERATIONS.
- 5. ALL SURFACES SHALL BE RESTORED TO THEIR ORIGINAL OR BETTER CONDITION AT THE COMPLETION OF CONSTRUCTION. EXISTING CONCRETE SUCH AS SIDEWALK, CURB, AND GUTTER SHALL BE REMOVED TO LIMITS MARKED IN FIELD BY THE ENGINEER. ALL REMOVAL MATERIALS SHALL BE DISPOSED OF OFF SITE AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL EXERCISE CAUTION WHEN WORKING ON PRIVATE PROPERTY.
- 6. AT LOCATIONS WHERE NEW UNDERGROUND FACILITIES CROSS EXISTING FACILITIES THE CONTRACTOR SHALL EXPOSE THE EXISTING FACILITY AND VERIFY THAT SUFFICIENT HORIZONTAL AND VERTICAL CLEARANCE EXISTS FOR THE NEW FACILITY TO BE CONSTRUCTED IN SUBSTANTIAL COMPLIANCE WITH THE PLANS. AT LOCATIONS WHERE NEW UNDERGROUND FACILITIES ARE TO BE CONNECTED TO EXISTING FACILITIES THE CONTRACTOR SHALL EXPOSE THE EXISTING FACILITY AND VERIFY THAT THE CONNECTION CAN BE MADE AS SHOWN ON THE PLANS. THIS VERIFICATION SHALL BE PERFORMED PRIOR TO ANY CONSTRUCTION. ANY CONFLICTS SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AS SOON AS THEY ARE DISCOVERED.
- 7. ALL DIMENSIONS TO CURBS OR CURB AND GUTTERS ARE TO THE FRONT FACE OF CURB UNLESS NOTED OTHERWISE ON THE DRAWINGS.
- 8. EXISTING DRAINAGE FACILITIES, OR INTERIM ENGINEER APPROVED ALTERNATIVES, SHALL BE KEPT IN SERVICE AT ALL TIMES DURING CONSTRUCTION. CONTRACTOR SHALL COMPLY WITH ALL REQUIREMENTS OF THE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) OF THE SUPPLEMENTAL GENERAL PROVISIONS OF THE SOLICITATION DOCUMENTS FOR SPARKS ALLEY WAY IMPROVEMENTS.
- 9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING ROADS, BUILDINGS OR OTHER STRUCTURES RESULTING FROM HIS CONSTRUCTION ACTIVITIES. REPAIRS SHALL BE MADE TO THE SATISFACTION OF THE CITY OF SPARKS, THE PROPERTY OWNERS, AND THE ENGINEER AT NO ADDITIONAL COST.
- 10. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF DISCREPANCIES BETWEEN THE INFORMATION SHOWN ON THESE DRAWINGS AND THE CONDITIONS EXISTING IN THE FIELD. THE CONTRACTOR SHALL COMPARE ALL DRAWINGS AND VERIFY THE FIGURES BEFORE STARTING THE WORK AND WILL BE RESPONSIBLE FOR ANY ERRORS WHICH MIGHT HAVE BEEN AVOIDED THEREBY. IF THE CONTRACTOR FAILS TO NOTIFY THE OWNER OR THEIR REPRESENTATIVE IN A TIMELY MANNER OF ANY APPARENT ERROR OR OMISSION ON THE PLANS OR SPECIFICATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING WORK INCORRECTLY DONE AT THE CONTRACTOR'S <u>EXPENSE.</u>
- 11. THE USE OF POTABLE WATER FROM THE PUBLIC WATER SYSTEM FOR CONSTRUCTION PURPOSES IS PROHIBITED. CONSTRUCTION WATER USED FOR COMPACTION AND DUST CONTROL SHALL BE OBTAINED FROM THE RENO-SPARKS SEWAGE TREATMENT PLANT AT 8500 CLEAN WATER WAY, RENO NEVADA, TMWA'S TRUCK FILL STATIONS, OR ANOTHER APPROVED SOURCE.
- 12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT ALL MANHOLE RIMS AND ANY EXISTING UTILITY COVERS WITHIN THE CONSTRUCTION LIMITS ARE SET FLUSH WITH THE NEW FINISH
- 13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING STAGING AREA LOCATIONS. CONTRACTOR SHALL OBTAIN WRITTEN APPROVAL PRIOR TO USING A STAGING AREA. THE CONTRACTOR SHALL OBTAIN ANY PERMITS FROM THE CITY OF SPARKS THAT ARE REQUIRED FOR STOCKPILING/PROCESSING MATERIALS.
- 14. PAYMENT FOR WORK SHOWN ON THESE PLANS EITHER SPECIFIED OR INFERRED, BUT NOT INCLUDED IN THE BID PROPOSAL, SHALL BE CONSIDERED AS INCLUDED IN THE PRICE PAID FOR OTHER ITEMS OF WORK.
- 15. DURING THE ENTIRE DURATION OF THIS CONSTRUCTION CONTRACT, THE CONTRACTOR SHALL IMPLEMENT STRINGENT DUST CONTROL MEASURES IN ACCORDANCE WITH THE TERMS OF THE APPROVED DUST CONTROL PERMIT AND WASHOE COUNTY HEALTH DEPARTMENT RULES AND REGULATIONS. THE CONTRACTOR IS REQUIRED TO SUPPRESS DUST AT ALL TIMES, 24 HOURS A DAY, SEVEN (7) DAYS A WEEK, REGARDLESS OF WHEN CONSTRUCTION ACTIVITIES ARE OCCURRING.
- 16. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRS TO EXISTING LANDSCAPING DAMAGED BY OR THROUGH CONSTRUCTION ACTIVITIES. REPAIRS SHALL BE MADE TO THE SATISFACTION OF THE ENGINEER AND OWNER. THERE WILL BE NO DIRECT PAYMENT FOR THIS WORK.
- 17. CITY OF SPARKS STANDARD DETAILS SHALL APPLY EXCEPT WHERE OTHERWISE NOTED ON THE PLANS.
- 18. BEFORE ANY WORK IS STARTED IN THE STREET RIGHT-OF-WAY, THE CONTRACTOR SHALL INSTALL ADVANCED WARNING SIGNS FOR THE CONSTRUCTION ZONE. ALL CONSTRUCTION SIGNING, BARRICADING, AND TRAFFIC DELINEATION SHALL CONFORM TO THE "NEVADA DEPARTMENT OF TRANSPORTATION STANDARD PLANS FOR ROAD AND BRIDGE CONSTRUCTION" — CURRENT EDITION AND TO THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" - CURRENT EDITION AND BE APPROVED BY THE CITY OF SPARKS.
- 19. PROTECTION AND REPLACEMENT OF ALL SURVEY MONUMENTS OR PROPERTY STAKES NOT DELINEATED ON THE CONTRACT DRAWINGS SHALL BE THE CONTRACTOR'S RESPONSIBILITY. DAMAGED OR REMOVED MONUMENTS AND/OR PROPERTY STAKES SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

LEGEND ----SS----- SANITARY SEWER S)—SS- SANITARY SEWER MANHOLE DROP INLET UTILITY POLE UTILITY POLE ANCHOR WATER LINE WATER METER WATER VALVE FIRE HYDRANT GAS LINE GAS VALVE EFFLUENT WATER LINE EFFLUENT WATER LINE MANHOLE OVERHEAD POWER UNDERGROUND COMMUNICATIONS TRAFFIC SIGNAL POLE PULL BOX STORM DRAIN STORM DRAIN MANHOLE (EXISTING) STORM DRAIN MANHOLE (PROPOSED) STORM DRAIN FLARED END SECTION EX. CATCH BASIN CURB & GUTTER CONTROL POINT BENCH MARK TELEPHONE MANHOLE TELEPHONE LINE

ELECTRIC FACILITIES (MANHOLE)

UTILITY POLE W/ LIGHT

LIGHT POLE

OOOO GUARDRAIL FENCE

BOLLARD

— — PROPERTY LINE

CENTERLINE RIGHT OF WAY

GRADE BREAK

— · · · — FLOWLINE

NOTE: ALL SYMBOLS OR ABBREVIATIONS MAY NOT BE USED ON PLANS

ABBREVIATIONS

ASPHALTIC CEMENT ALGEBRAIC DIFFERENCE ANGLE POINT ASPHALT PAVEMENT PATH AIR RELEASE VALVE BEGINNING OF CURVE BOTTOM OF FOOTING
BACK FACE OF CURB
BEGINNING OF VERTICAL CURVE STATION BACK OF SIDEWALK CATCH BASIN ÇorCL CMP ORRUGATED METAL PIPE CONCRETE CONSTRUCT CONC. CONST. DROP INLET DUCTILE IRON PIPE END OF CURVE EXISTING GROUND ELEV. ELEVATION EDGE OF PAVEMENT
END OF VERTICAL CURVE ELEVATION
END OF VERTICAL CURVE STATION EXISTING EXISTING FINISH FLOOR FRONT FACE OF CURB FINISH GRADE FIRE HYDRANT FLOWLINE LANGED LUSH VALVE GRADE BREAK HORIZONTAL HEAD WALL INVERT ELEVATION RATE OF VERTICAL CURVATURE LENGTH LATERAL LINEAL FEET LOW POINT MAXIMUM DRY DENSITY MIN. MINIMUM MECHANICAL JOINT MPOC MID POINT OF CURVE POINT OF CURVATURE PORTLAND CEMENT CONCRETE
POINT OF COMPOUND CURVATURE
POINT OF INTERSECTION POINT ON CURVE POINT ON TANGENT PEDESTRIAN PUSH BUTTON
POINT OF REVERSE CURVATURE
POINT OF TANGENCY POLYVINYL CHLORIDE POINT OF VERTICAL INTERSECTION REINFORCED CONCRETE PIPE REFERENCE RETURN RADIUS POINT RIGHT RIGHT OF WAY STORM DRAIN STORM DRAIN MANHOLE SQUARE FEET SANITARY SEWER SANITARY SEWER MANHOLE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION SHARED USE PATH SIDEWALK TOP OF CURB TOP OF DEPRESSED CURB TOP OF PAVEMENT VERT. VERTICAL CURVE VERTICAL POINT OF INTERSECTION

INDEX OF SHEETS							
SHEET NO.	SHEET NAME	SHEET DESCRIPTION					
1	C-0	COVER SHEET					
2	C-1	GENERAL NOTES, LEGEND, AND ABBREVIATIONS					
3	SI-1	PROPOSED SECTIONS OF IMPROVEMENT					
4	SP-1	SITE PLAN					
5	HC-1	HORIZONTAL CONTROL PLAN					
6	PP-1	PLAN AND PROFILE					
7	DT-1	DETAILS					
8	DT-2	DETAILS					

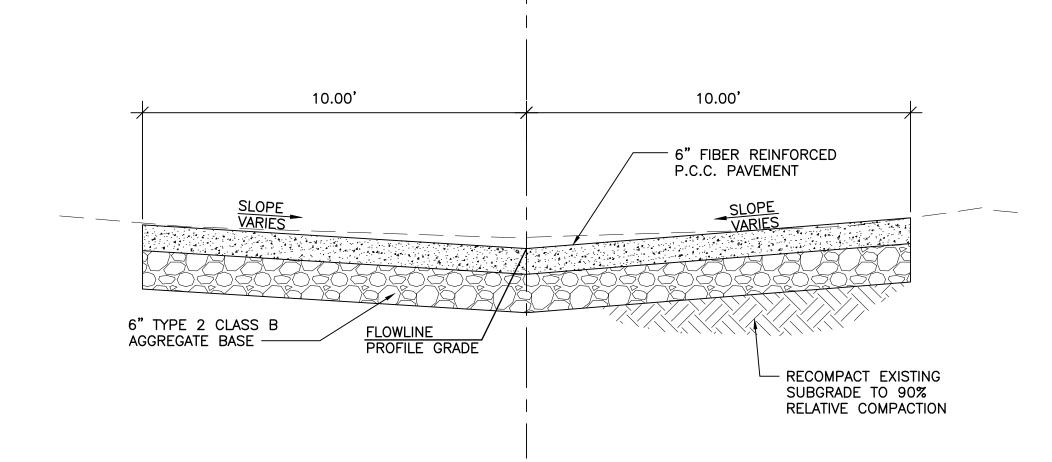
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Project Number: 180101424

File Name: 01424 SA-B C1.dwa JAM CJA 19.11.15

Drawing No. C-1 Revision Sheet

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PROPOSED SECTION OF IMPROVEMENT - FLOWLINE AT CENTERLINE ALLEY "B" STA. 9+98.49 TO STA. 14+00.92

NOTES:

- 1. PCC PAVEMENT SHALL BE JOINTED AT 6'-8" ON CENTER FOR LONGITUDINAL JOINTS AND 8'-0" ON CENTER FOR TRANSVERSE JOINTS. JOINTS TO BE CUT A MINIMUM OF 4 HOURS TO A MAXIMUM OF 12 HOURS AFTER CONCLUSION OF BRUSH FINISHING.
- 2. SEE SPECIFICATIONS FOR MIX DESIGN.
- 3. AT STA. 9+98.49 AND STA. 14+00.92. INTERFACE WITH THE EXISTING DRIVEWAY SHALL BE DOWELED IF THE EXISTING DRIVEWAY THICKNESS IS 6" OR GREATER. SEE DETAIL 7/DT-2. NO DIRECT PAYMENT.
- 4. FIBER REINFORCEMENT SHALL BE TUF STRAND.

ALLEY "B" LIP/EP ELEVATIONS AND SLOPE									
LT. OFFSET	LT. ELEV	LT. SLOPE	STATION	RT. SLOPE	RT. ELEV	RT. OFFSET			
10.00	4418.35'	0.93%	10+25.00	1.47%	4418.40'	10.00			
10.00	4418.04'	1.26%	10+25.00	3.02%	4418.21'	10.00			
10.00	4417.90'	3.32%	10+50.00	3.24%	4417.89'	10.00			
10.00	4417.58'	0.82%	10+75.00	2.26%	4417.72'	10.00			
10.00	4417.67'	2.47%	11+00.00	3.40%	4417.76'	10.00			
10.00	4417.74'	3.94%	11+25.00	6.14%	4417.96'	10.00			
10.00	4417.70'	5.05%	11+50.00	4.50%	4417.64'	10.00			
10.00	4417.61'	2.28%	11+75.00	2.38%	4417.62'	10.00			
10.00	4417.71'	1.48%	12+00.00	2.66%	4417.83'	10.00			
10.00	4417.92'	2.04%	12+25.00	1.57%	4417.88'	10.00			
10.00	4417.99'	1.98%	12+50.00	1.47%	4417.94'	10.00			
10.00	4417.93'	0.58%	12+75.00	3.71%	4418.24	10.00			
10.00	4418.17	2.28%	13+00.00	2.10%	4418.16'	10.00			
10.00	4418.44	3.01%	13+25.00	3.26%	4418.47'	10.00			
10.00	4418.62'	2.84%	13+50.00	2.17%	4418.56'	10.00			
10.00	4418.58'	2.01%	13+75.00	2.33%	4418.61'	10.00			
10.00	4418.33'	1.60%	14+00.00	0.70%	4418.24	10.00			

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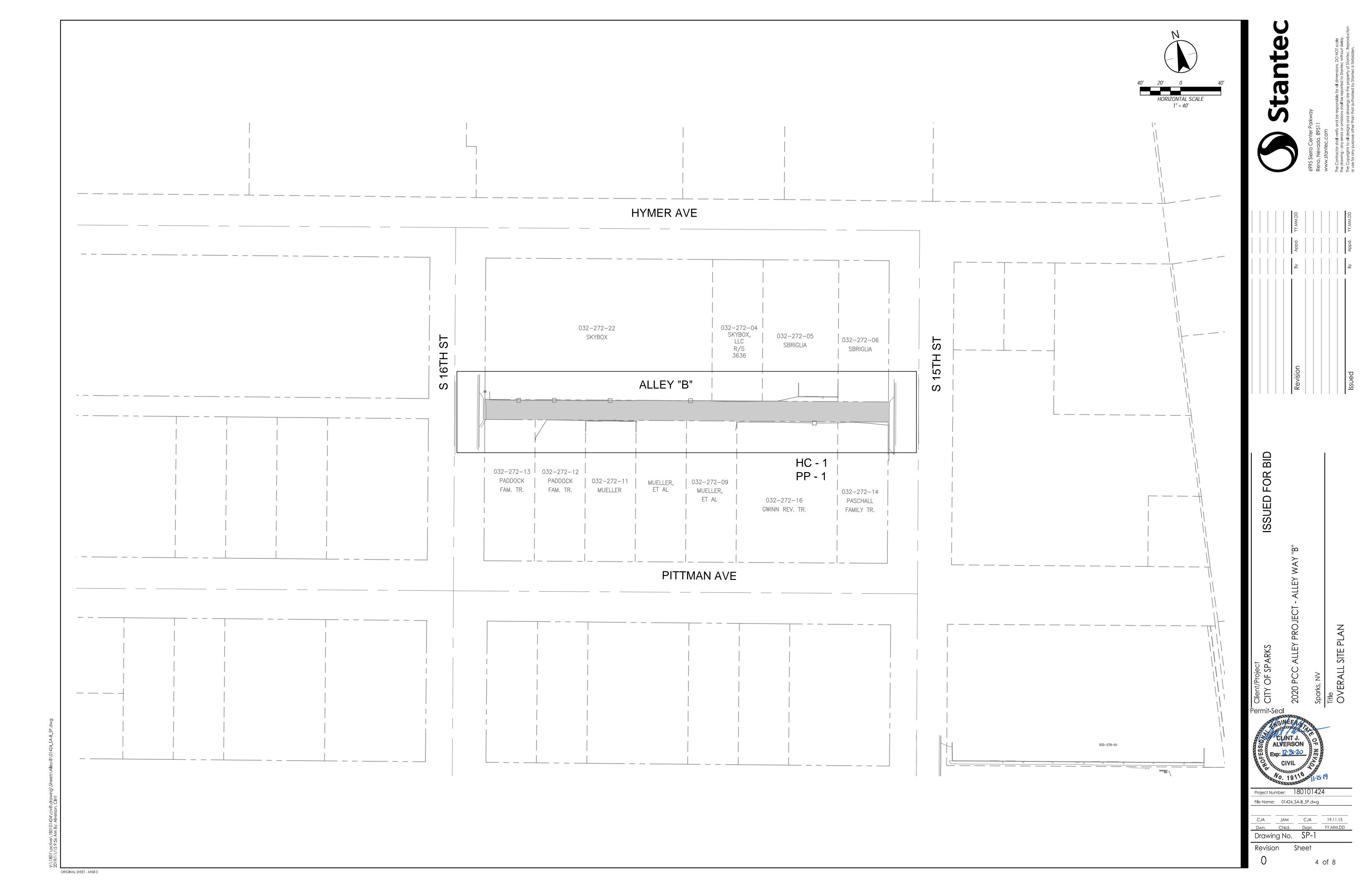
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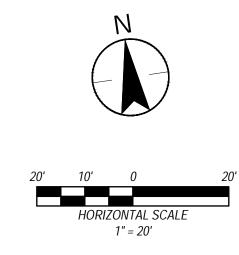
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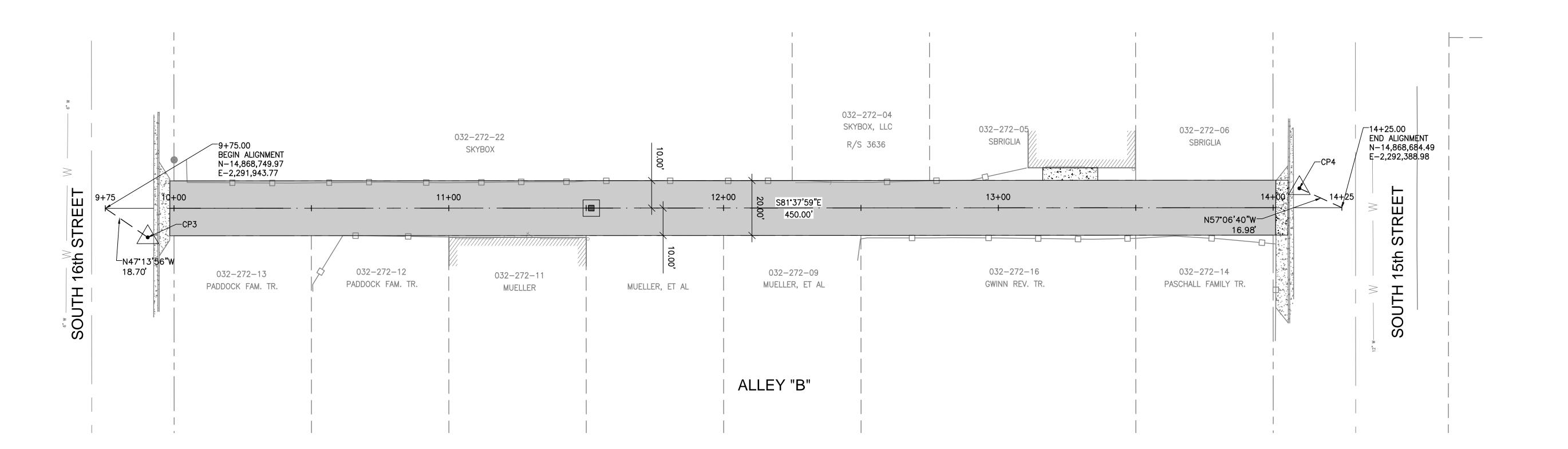
Drawing No. SI-1

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SURVEY CONTROL POINT TABLE										
POINT	NORTHING	EASTING	ELEVATION	DESCRIPTION	STATION	OFFSET				
CP3	14,868,737.27	2,291,957.51	4418.46	SET MAG WP - LOW	9+90.43	10.57' RT.				
CP4	14,868,693.72	2,292,374.72	4417.87	SET MAG WP - DOWN	14+09.55	7.05' LT.				



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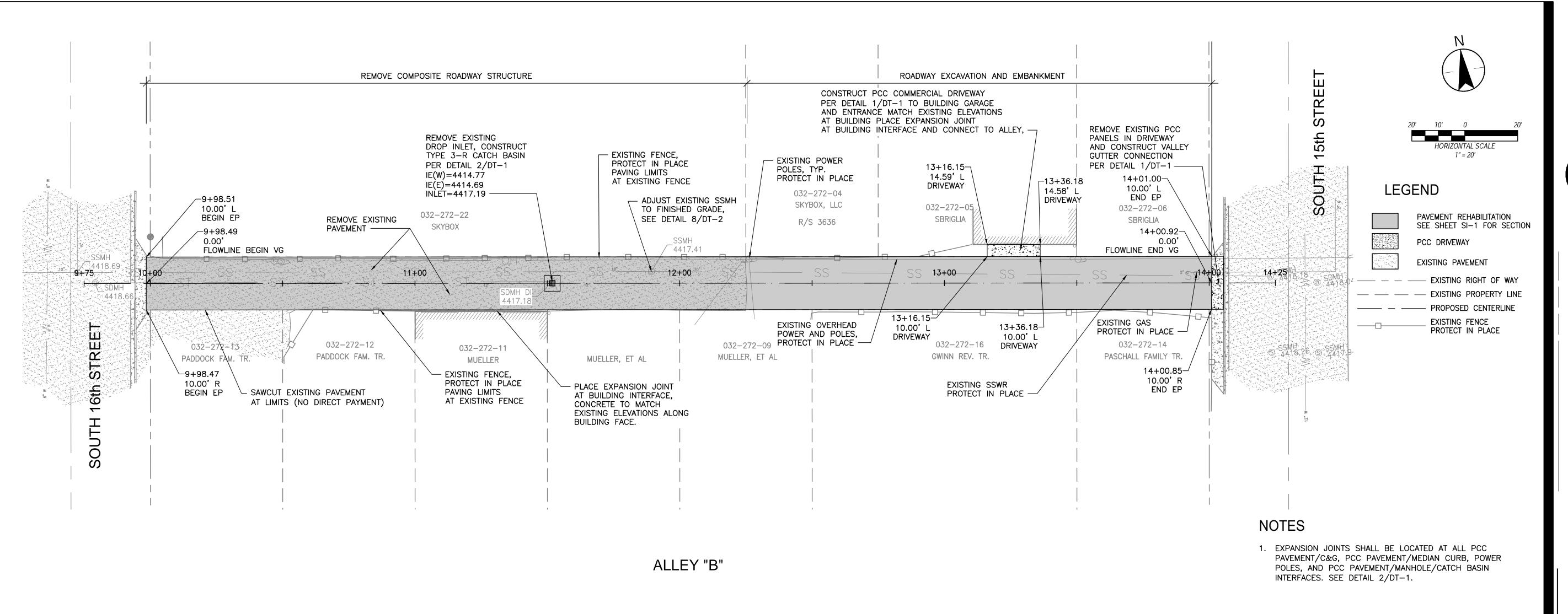
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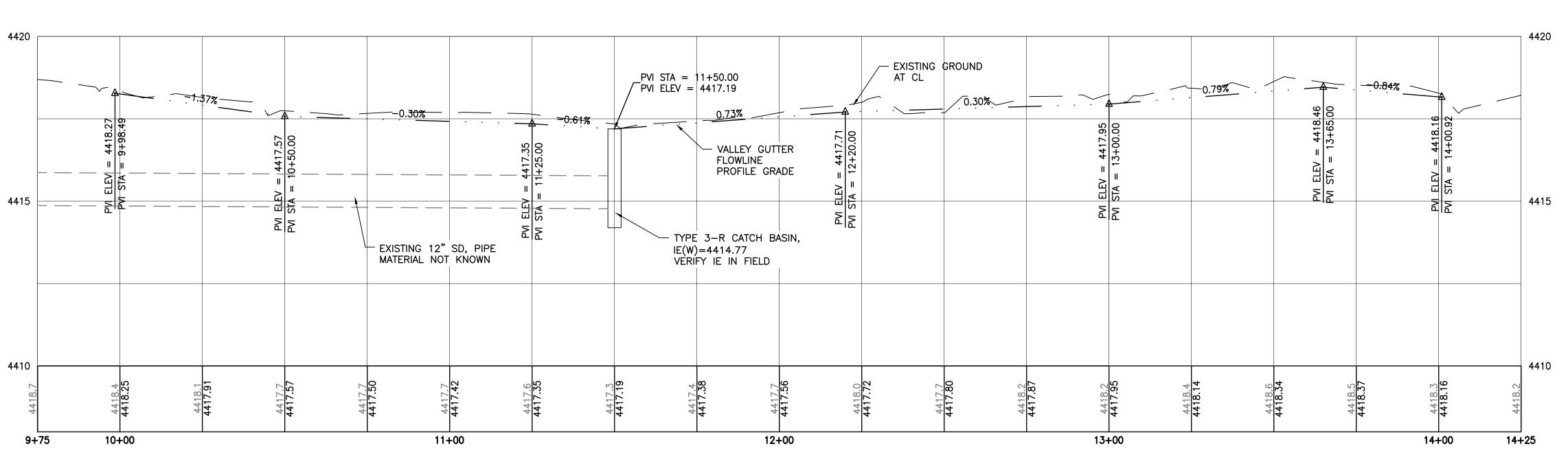
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Dwn. Chkd. Dsgn. YY.MM.DD

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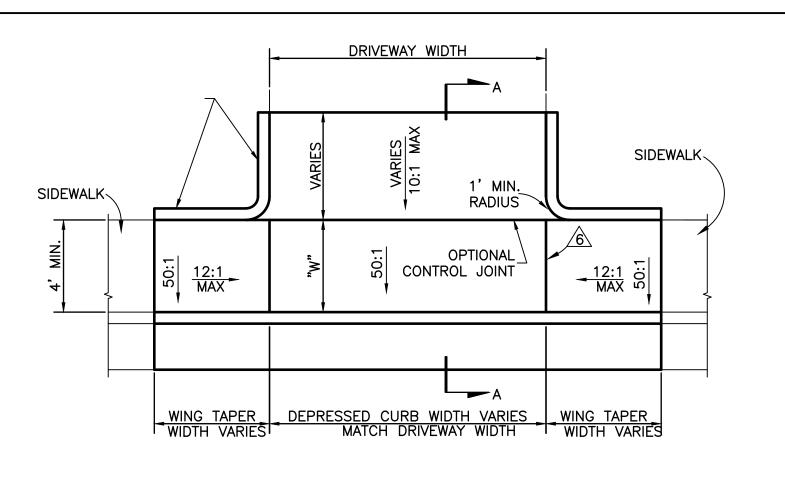
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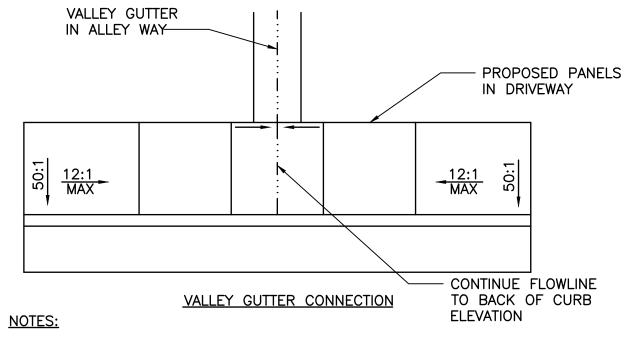
Revision

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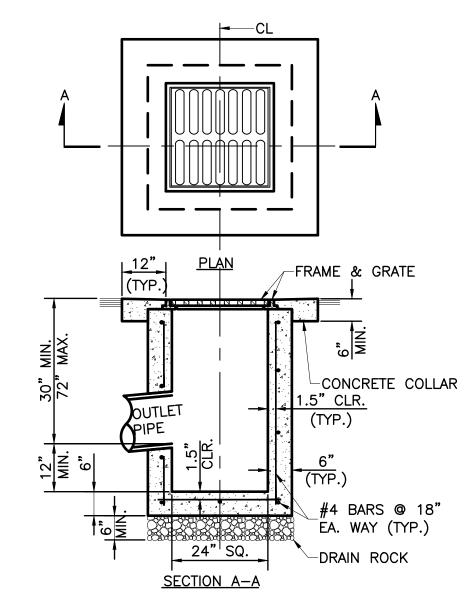


4' MIN. **EXISTING** _ P.C.C GUTTER 1" LIP 6" P.C.C¬ DRIVEWAY OR AC BEVELED @ 1:1 ASPHALT PAVEMENT 8" MIN. AGGREGATE BASE 8" MIN. AGGREGATE BASE -COMPACTED TO A MINIMUM COMPACTED TO A MINIMUM-95% RELATIVE COMPACTION 95% RELATIVE COMPACTION SECTION A-A

<u>PLAN</u>



- 1. SEE SPECIFICATIONS FOR CONCRETE MIX DESIGN.
- 2. AGGREGATE BASE MATERIAL UNDER DRIVEWAYS AND SIDEWALKS SHALL BE TYPE 2, CLASS B CRUSHED AGGREGATE BASE. ALL MATERIALS SHALL CONFORM TO SSPWC SECTION 200.
- 3. COMMERCIAL DRIVEWAYS MAY BE POURED MONOLITHIC WITH CURB AND GUTTER. COMMERCIAL DRIVEWAYS TO HAVE #4 BARS AT 18" ON CENTER LONGITUDINAL & TRANSVERSE EXTENDING INTO GUTTER PAN AND DRIVEWAY WINGS. MINIMUM 2" CONCRETE COVER FOR ALL REINFORCING BARS. WHEN COMMERCIAL DRIVEWAY APPROACH AND CURB & GUTTER IS POURED SEPARATELY, IT SHALL BE REQUIRED FOR EACH REINFORCING BAR TO BE DOWELED INTO ADJACENT CURB & GUTTER. DOWELS SHALL #4 REBAR, PENETRATE INTO CURB & GUTTER MINIMUM OF 6", SPACED AT 18" ON CENTER AND BE SECURELY TIED TO THE DRIVEWAY APPROACH REINFORCING.
- 4. IF JOINT EXISTS WITHIN 4 FEET OF DRIVEWAY, REMOVE SIDEWALK AND CURB AND GUTTER TO THAT JOINT.
- 5. ALL ADJACENT CONCRETE REMOVAL SHALL BE TO NEAT SAW CUT LINES AT RIGHT ANGLES. DOWEL INTO EXISTING ADJACENT CONCRETE DRIVEWAY APPROACH OR SIDEWALK WITH (2) No. 4 REINFORCEMENT BARS EQUALLY SPACED ACROSS WIDTH "W". DOWELS SHALL PENETRATE A MINIMUM OF 4" INTO EXISTING CONCRETE.
- WEAKENED PLANE JOINTS SHALL BE CONSTRUCTED AT 5 FT INTERVALS AND IN ACCORDANCE WITH SECTION 312 OF THE SSPWC.



NOTES:

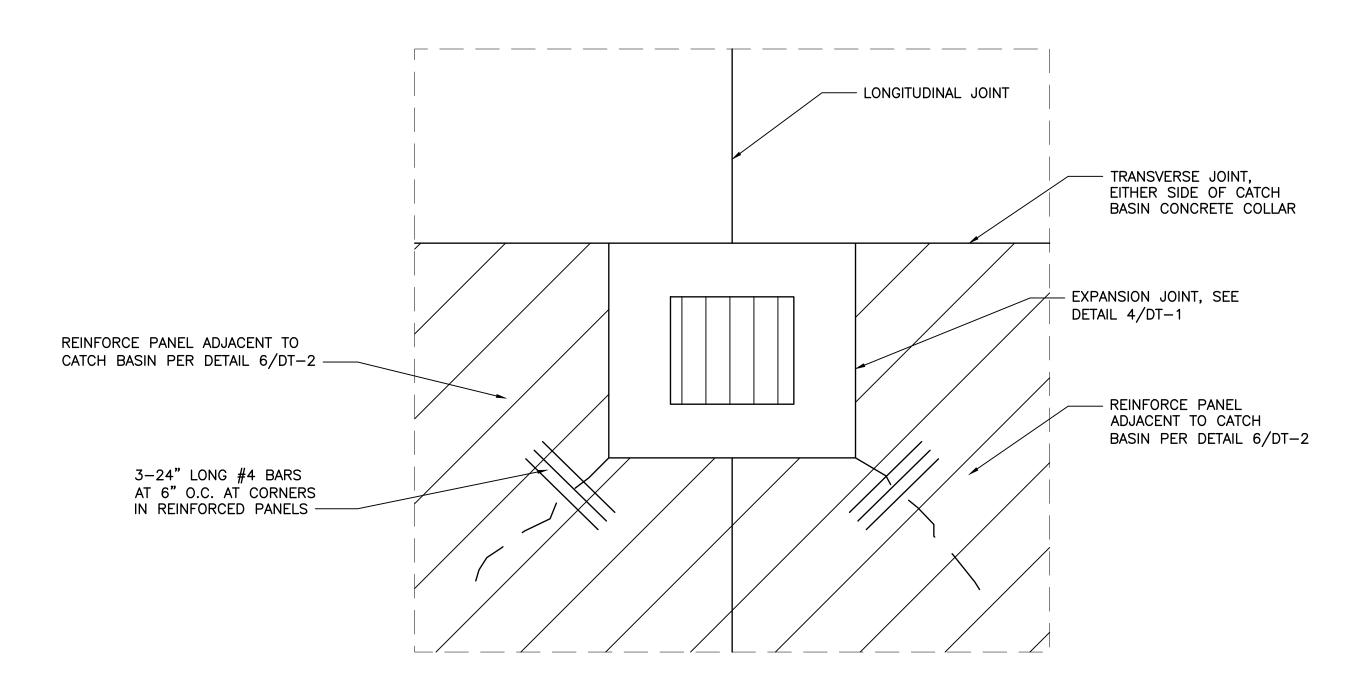
- 1. SEE SPECIFICATIONS FOR CONCRETE MIX DESIGN.
- 2. REINFORCING STEEL SHALL BE GRADE 40 AND HAVE 1.5" CLEAR COVER.
- 3. CONCRETE STRUCTURE MAY BE A PRE—CAST CONCRETE UNIT. BASE OF PRE—CAST CONCRETE UNIT SHALL BE PLACED ON 6" COMPACTED DRAIN ROCK.
- 4. FRAME & GRATE SHALL BE D&L I-9226 OR APPROVED EQUAL.
- 5. CATCH BASIN SHALL BE TRAFFIC-RATED AND USED ONLY AT LOW POINTS IN ALLEYS OR PARKING AREAS.
- 6. ALL CATCH BASINS, PUBLIC OR PRIVATE, SHALL BE PROVIDED WITH A "SUR-TRAP" OIL/WATER SEPARATOR OR APPROVED EQUAL.

<u>2</u> DT-1

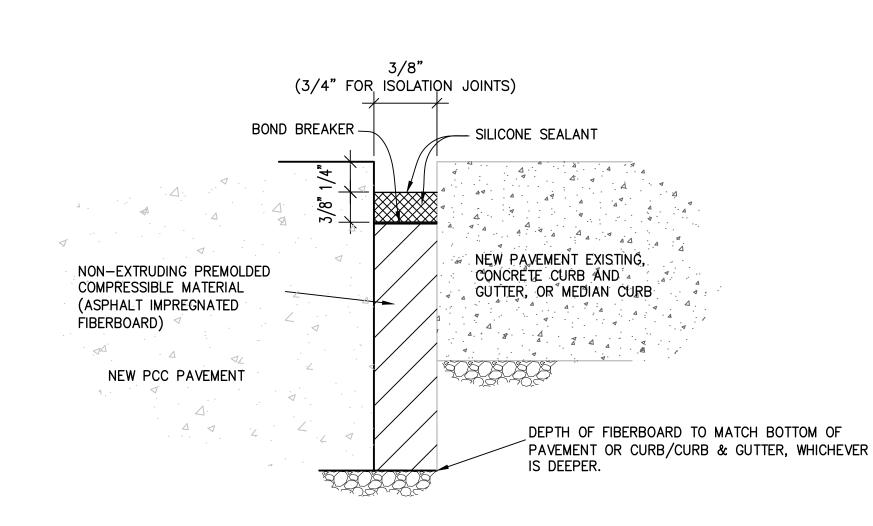
CATCH BASIN TYPE 3-R DRAIN INLET

NTS

1 COMMERCIAL DRIVEWAY
DT-1 NTS



3 CATCH BASIN REINFORCEMENT DETAIL



4 TYPICAL EXPANSION JOINT

ISSUED FOR BID

"B"

Revision

Issued

ALLEY PROJECT - ALLEY WAY

2020 PCC ALLEY F

Clint J.

Spark

Project Number: 180101424

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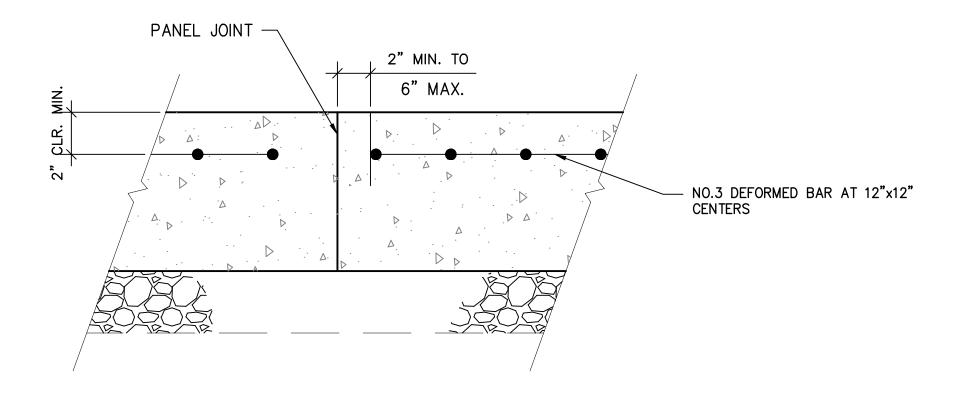
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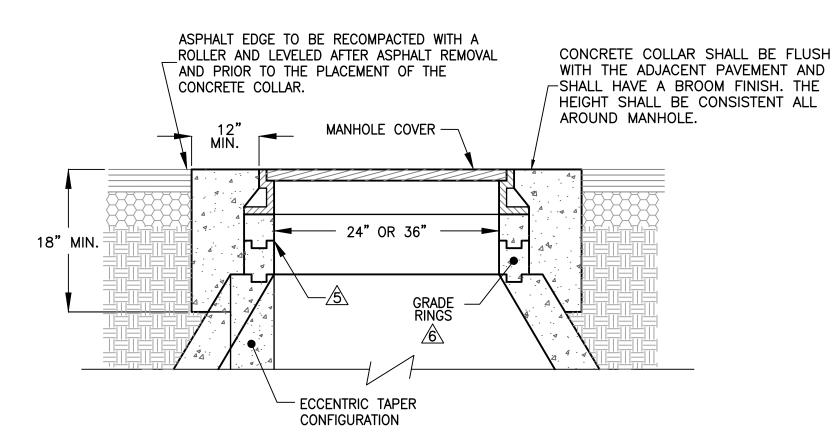
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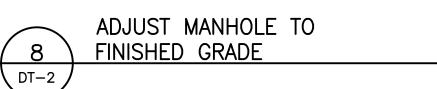
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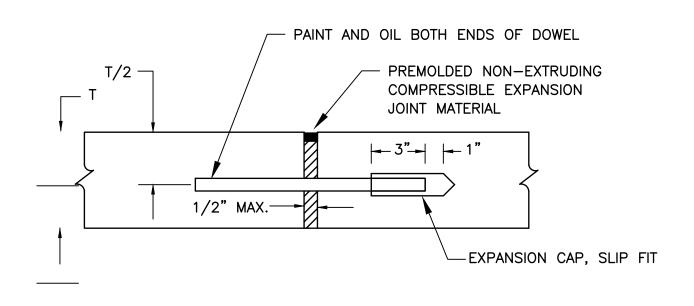






- 1. SEE SPECIFICATIONS FOR CONCRETE MIX DESIGN.
- 2. CIRCUMSTANCES MAY REQUIRE THE NEED FOR SPECIAL TYPES OF TOP OF MANHOLE CONFIGURATIONS SUCH AS FLAT TOP, ABOVE GROUND, ETC. AS DIRECTED BY THE OWNING AGENCY. DETAILED PLANS OF ANY SPECIAL TOP OF MANHOLE CONFIGURATIONS AND ASSOCIATED COLLARS MUST BE APPROVED BY THE ENGINEER.
- 3. IN UNPAVED AREAS, IT SHALL BE NECESSARY TO SET THE MANHOLE RIM APPROXIMATELY 6 INCHES ABOVE THE SURROUNDING AREA. INSTALL A 6 INCH THICK RING OF CONCRETE, TAPERED AT A 3:1 SLOPE, FROM THE TOP, OUTSIDE EDGE OF THE COLLAR TO THE EXISTING GROUND SURFACE.
- 4. MANHOLE LIDS SHALL NOT BE LOCATED IN GUTTER PANS, UNLESS OTHERWISE APPROVED BY THE OWNING
- 5. ALL GRADE RING JOINTS ARE TO BE GROUTED WITH NON-SHRINK GROUT CONFORMING TO SSPWC SECTION 202 "CEMENT AND RELATED MATERIALS.".
- 6. ALL GRADE RINGS SHALL BE PORTLAND CEMENT CONCRETE. PVC GRADE RINGS ARE NOT ALLOWED.





- 1. DOWEL BAR SPACING SHALL BE 15".
- 2. DOWEL BARS SHALL NOT BE PLACED WITHIN 15" OF LONGITUDINAL



