

A CITY OF SPARKS PROJECT

ISSUED FOR BID

CITY OF SPARKS PROJECT BID NO. 19/20-013 PUBLIC WORKS PROJECT NO. WA-2020-079 2020 PCB ALLEY PROJECT - ALLEY WAY "B" SPARKS, WASHOE COUNTY, NEVADA

CITY OF SPARKS

RONALD E. SMITH	MAYOR
DONALD ABBOTT	COUNCIL MEMBER WARD 1
ED LAWSON	COUNCIL MEMBER WARD 2
PAUL ANDERSON	COUNCIL MEMBER WARD 3
CHARLENE BYBEE	COUNCIL MEMBER WARD 4
KRISTOPHER DAHIR	COUNCIL MEMBER WARD 5
NEIL KRUTZ	CITY MANAGER



APPROVALS:




JON ERICSON, P.E., P.T.O.E.
CITY ENGINEER

11/15/19
DATE

PLANS PREPARED AND SUBMITTED BY:




CLINT J. ALVERSON, P.E.
PROJECT MANAGER

11/15/2019
DATE

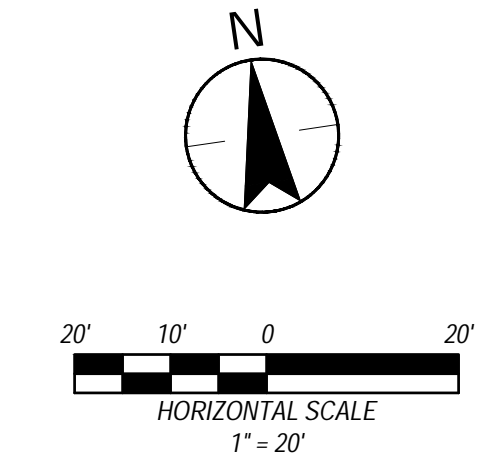
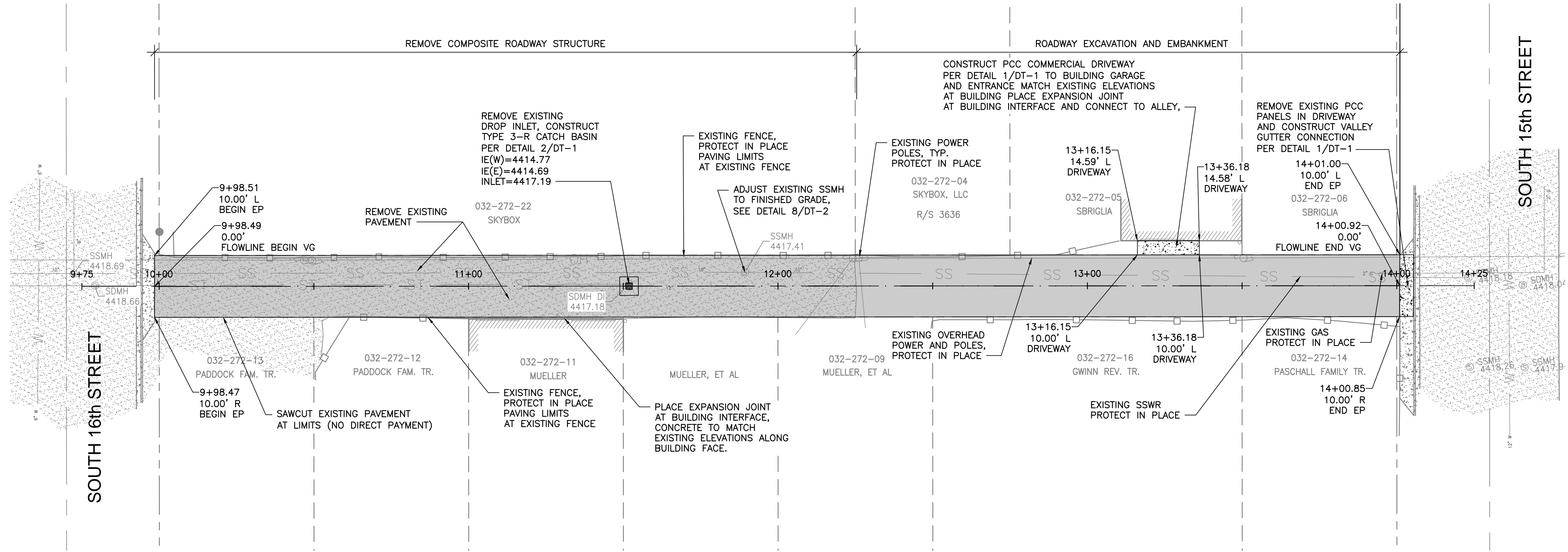
ENGINEER:



6995 Sierra Center Parkway
Reno, NV 89511
www.stantec.com



STANTEC PROJECT NO. 180101424
C-0



LEGEND

[Pattern]	PAVEMENT REHABILITATION SEE SHEET SI-1 FOR SECTION
[Pattern]	PCC DRIVEWAY
[Pattern]	EXISTING PAVEMENT
[Line]	EXISTING RIGHT OF WAY
[Line]	EXISTING PROPERTY LINE
[Line]	PROPOSED CENTERLINE
[Line]	EXISTING FENCE PROTECT IN PLACE

Stantec

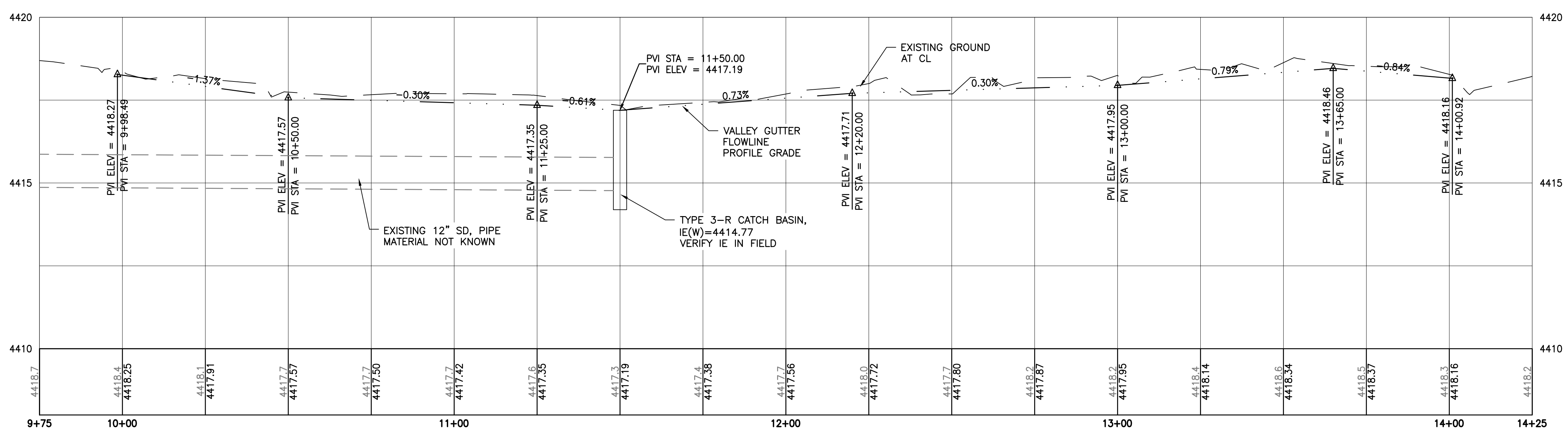
6995 Sierra Center Parkway
Reno, Nevada, 89511
www.stantec.com

The Contractor shall verify and be responsible for all dimensions, POC NOT scale. The drawing - any errors or omissions shall be reported to Stantec, without delay. The Copyright to all designs and drawings are the property of Stantec. Reproduction or use for any purpose other than that authorized by Stantec is forbidden.

STORM DRAIN LOCATION FOR IFC	By	YYMMDD
Revision	By	YYMMDD
Issued	By	YYMMDD

ALLEY "B"

- NOTES**
- EXPANSION JOINTS SHALL BE LOCATED AT ALL PCC PAVEMENT/C&G, PCC PAVEMENT/MEDIAN CURB, POWER POLES, AND PCC PAVEMENT/MANHOLE/CATCH BASIN INTERFACES. SEE DETAIL 2/DT-1.



V:\1811\181115\181115.dwg, South Spawning Streets, Alley B, 01424_SA-8_PP.dwg
 2018/11/15 15:57:53, ANS/D, ALV/J

ISSUED FOR BID

Client/Project
CITY OF SPARKS

Permit/Seal

2020 PCC ALLEY PROJECT - ALLEY WAY "B"

Sparks, NV

Title
PLAN AND PROFILE

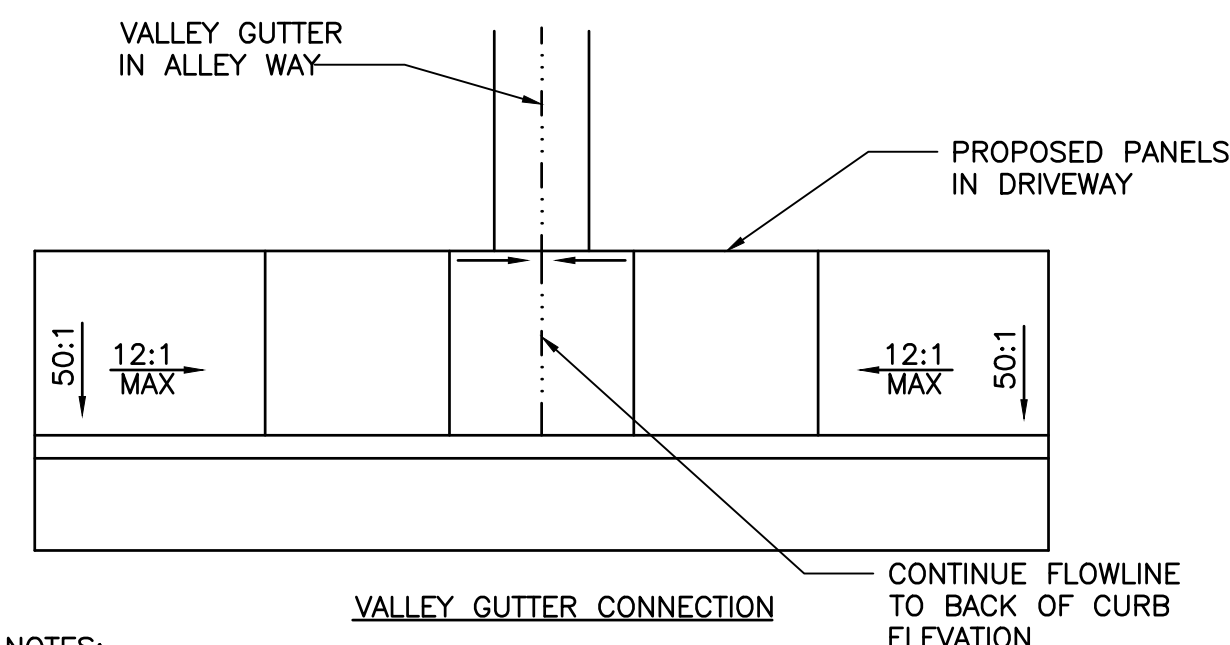
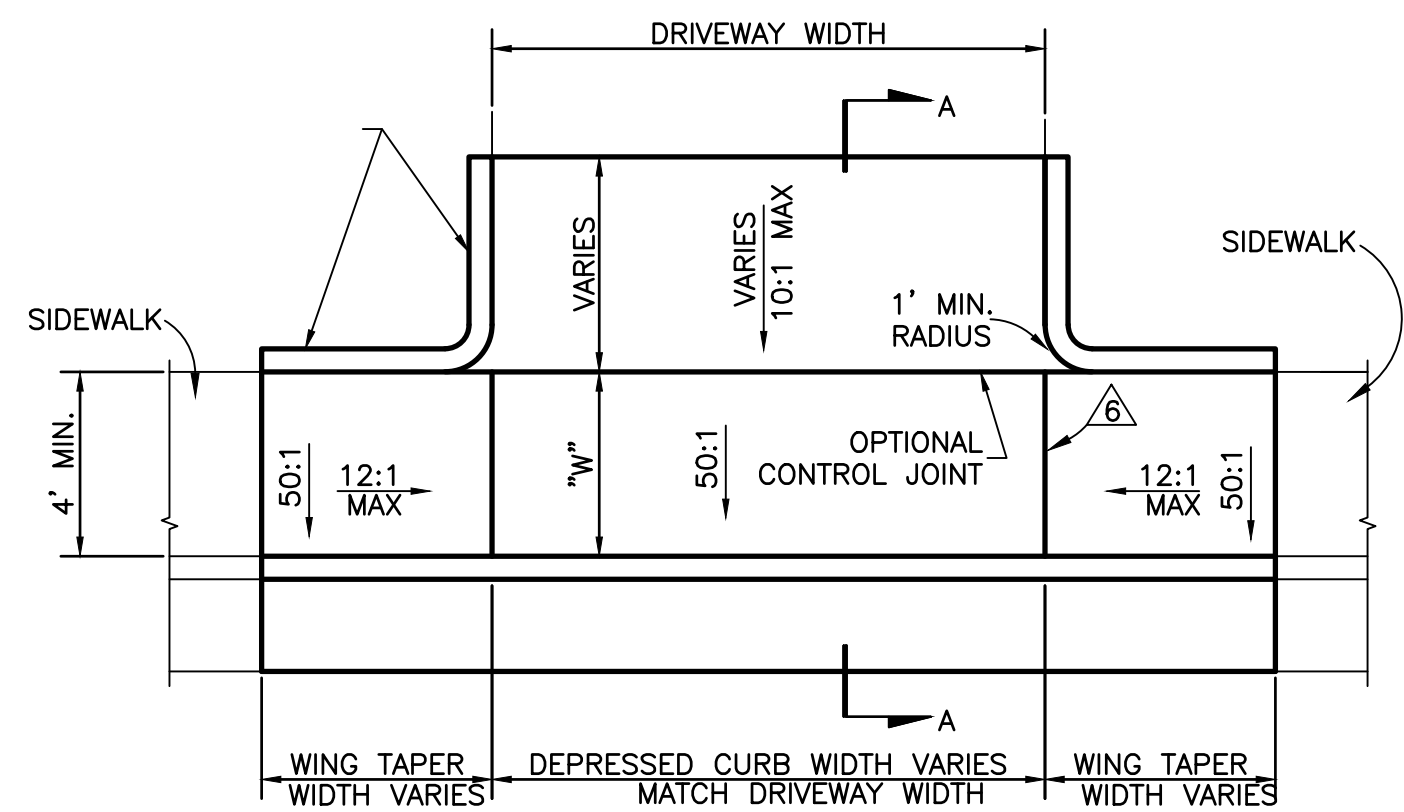
Project Number: 180101424
File Name: 01424_SA-8_PP.dwg

CJA JAM CJA 19.11.15
Dwn. Chkd. Dsgn. YYMMDD

Drawing No. PP-1
Revision Sheet

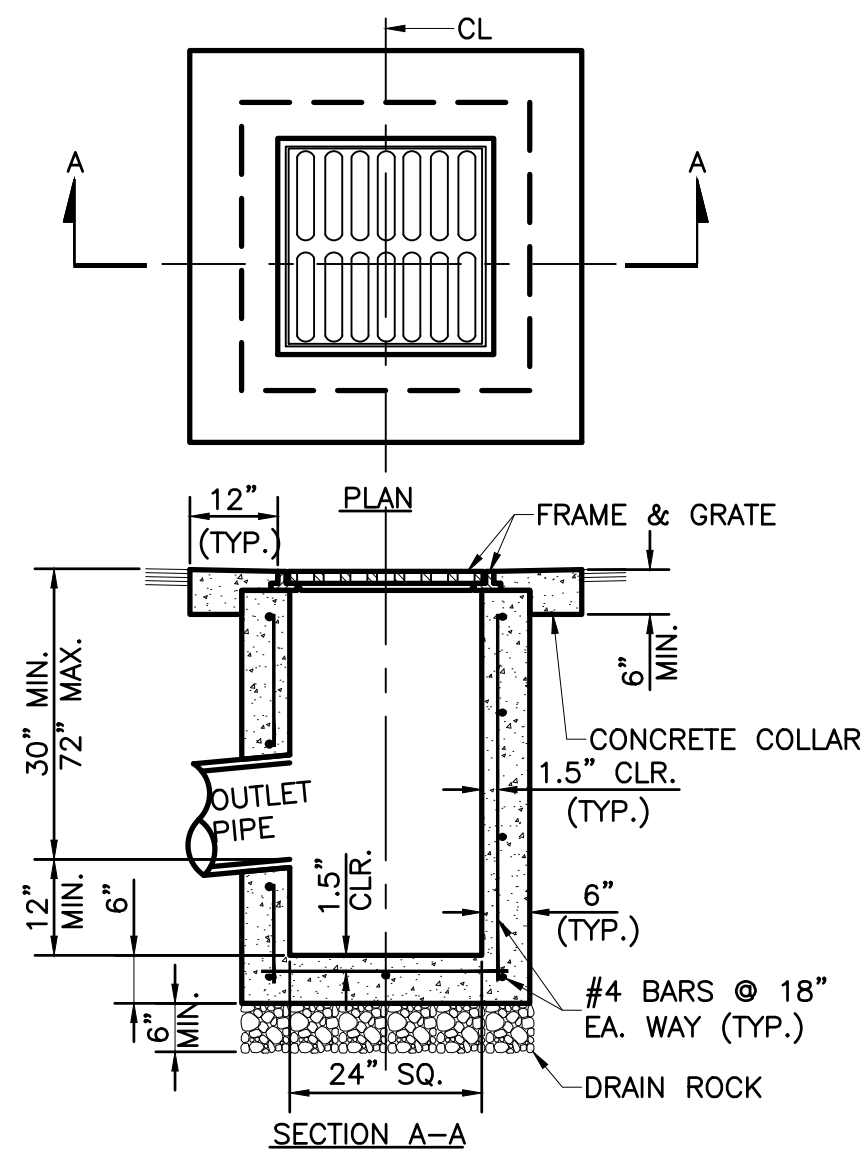
0 6 of 8

Professional Engineer Seal: CLINT J. ALVERSON, CIVIL, No. 18116, Exp. 12-31-20, 11-15-19

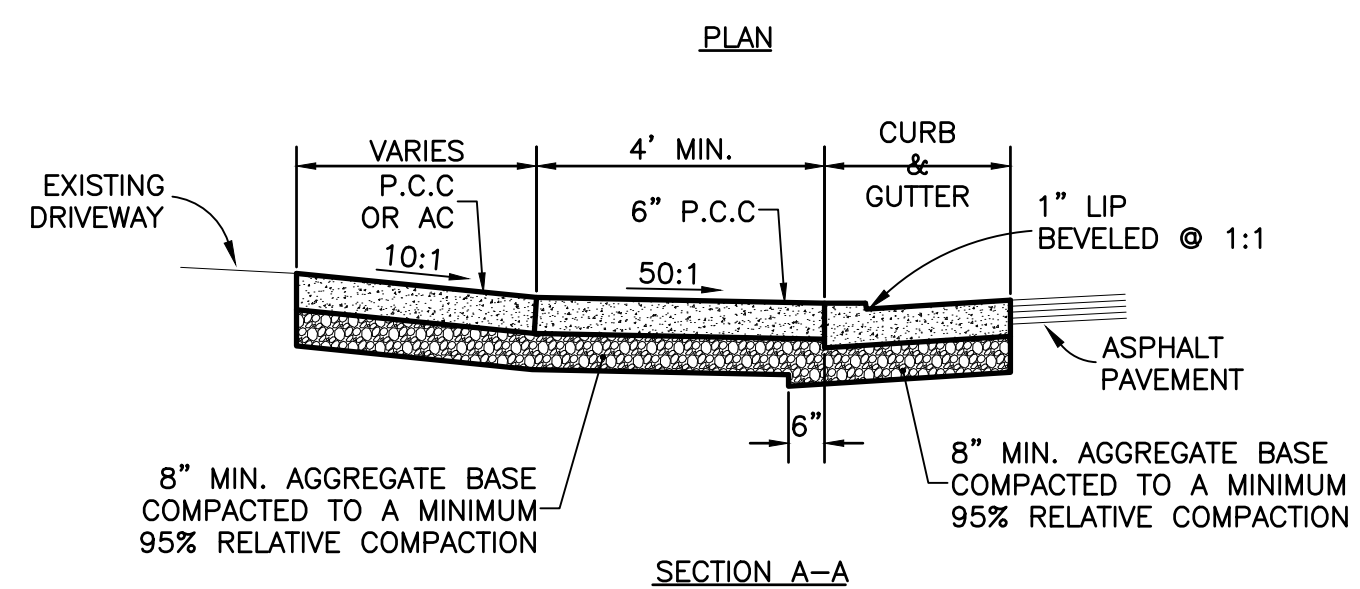


- NOTES:
- SEE SPECIFICATIONS FOR CONCRETE MIX DESIGN.
 - AGGREGATE BASE MATERIAL UNDER DRIVEWAYS AND SIDEWALKS SHALL BE TYPE 2, CLASS B CRUSHED AGGREGATE BASE. ALL MATERIALS SHALL CONFORM TO SSPWC SECTION 200.
 - COMMERCIAL DRIVEWAYS MAY BE POURED MONOLITHIC WITH CURB AND GUTTER. COMMERCIAL DRIVEWAYS TO HAVE #4 BARS AT 18" ON CENTER LONGITUDINAL & TRANSVERSE EXTENDING INTO GUTTER PAN AND DRIVEWAY WINGS. MINIMUM 2" CONCRETE COVER FOR ALL REINFORCING BARS. WHEN COMMERCIAL DRIVEWAY APPROACH AND CURB & GUTTER IS POURED SEPARATELY, IT SHALL BE REQUIRED FOR EACH REINFORCING BAR TO BE DOWELED INTO ADJACENT CURB & GUTTER. DOWELS SHALL #4 REBAR, PENETRATE INTO CURB & GUTTER MINIMUM OF 6", SPACED AT 18" ON CENTER AND BE SECURELY TIED TO THE DRIVEWAY APPROACH REINFORCING.
 - IF JOINT EXISTS WITHIN 4 FEET OF DRIVEWAY, REMOVE SIDEWALK AND CURB AND GUTTER TO THAT JOINT.
 - ALL ADJACENT CONCRETE REMOVAL SHALL BE TO NEAT SAW CUT LINES AT RIGHT ANGLES. DOWEL INTO EXISTING ADJACENT CONCRETE DRIVEWAY APPROACH OR SIDEWALK WITH (2) No. 4 REINFORCEMENT BARS EQUALLY SPACED ACROSS WIDTH "W". DOWELS SHALL PENETRATE A MINIMUM OF 4" INTO EXISTING CONCRETE.

WEAKENED PLANE JOINTS SHALL BE CONSTRUCTED AT 5 FT INTERVALS AND IN ACCORDANCE WITH SECTION 312 OF THE SSPWC.

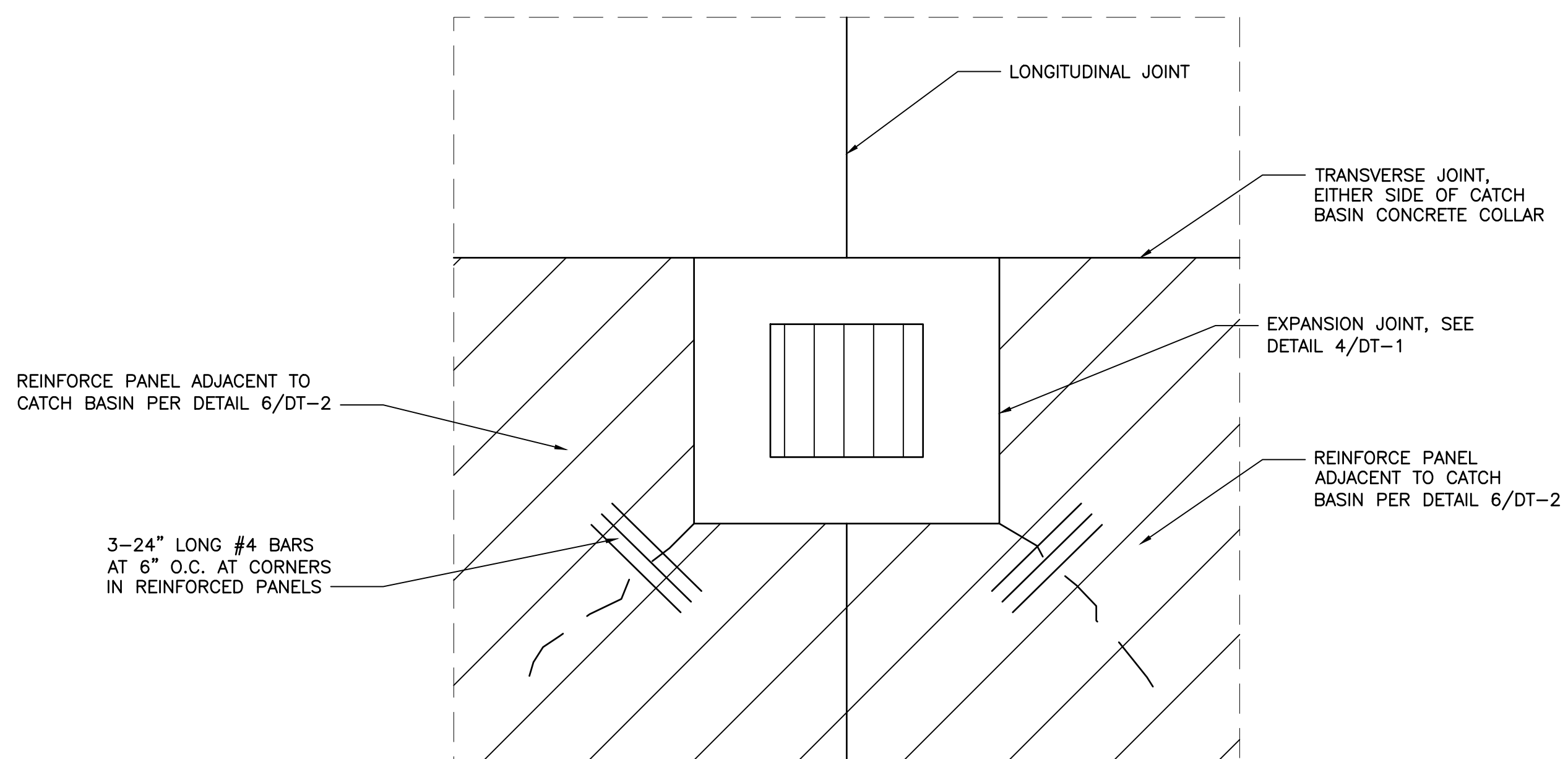


- NOTES:
- SEE SPECIFICATIONS FOR CONCRETE MIX DESIGN.
 - REINFORCING STEEL SHALL BE GRADE 40 AND HAVE 1.5" CLEAR COVER.
 - CONCRETE STRUCTURE MAY BE A PRE-CAST CONCRETE UNIT. BASE OF PRE-CAST CONCRETE UNIT SHALL BE PLACED ON 6" COMPACTED DRAIN ROCK.
 - FRAME & GRATE SHALL BE D&L I-9226 OR APPROVED EQUAL.
 - CATCH BASIN SHALL BE TRAFFIC-RATED AND USED ONLY AT LOW POINTS IN ALLEYS OR PARKING AREAS.
 - ALL CATCH BASINS, PUBLIC OR PRIVATE, SHALL BE PROVIDED WITH A "SUR-TRAP" OIL/WATER SEPARATOR OR APPROVED EQUAL.

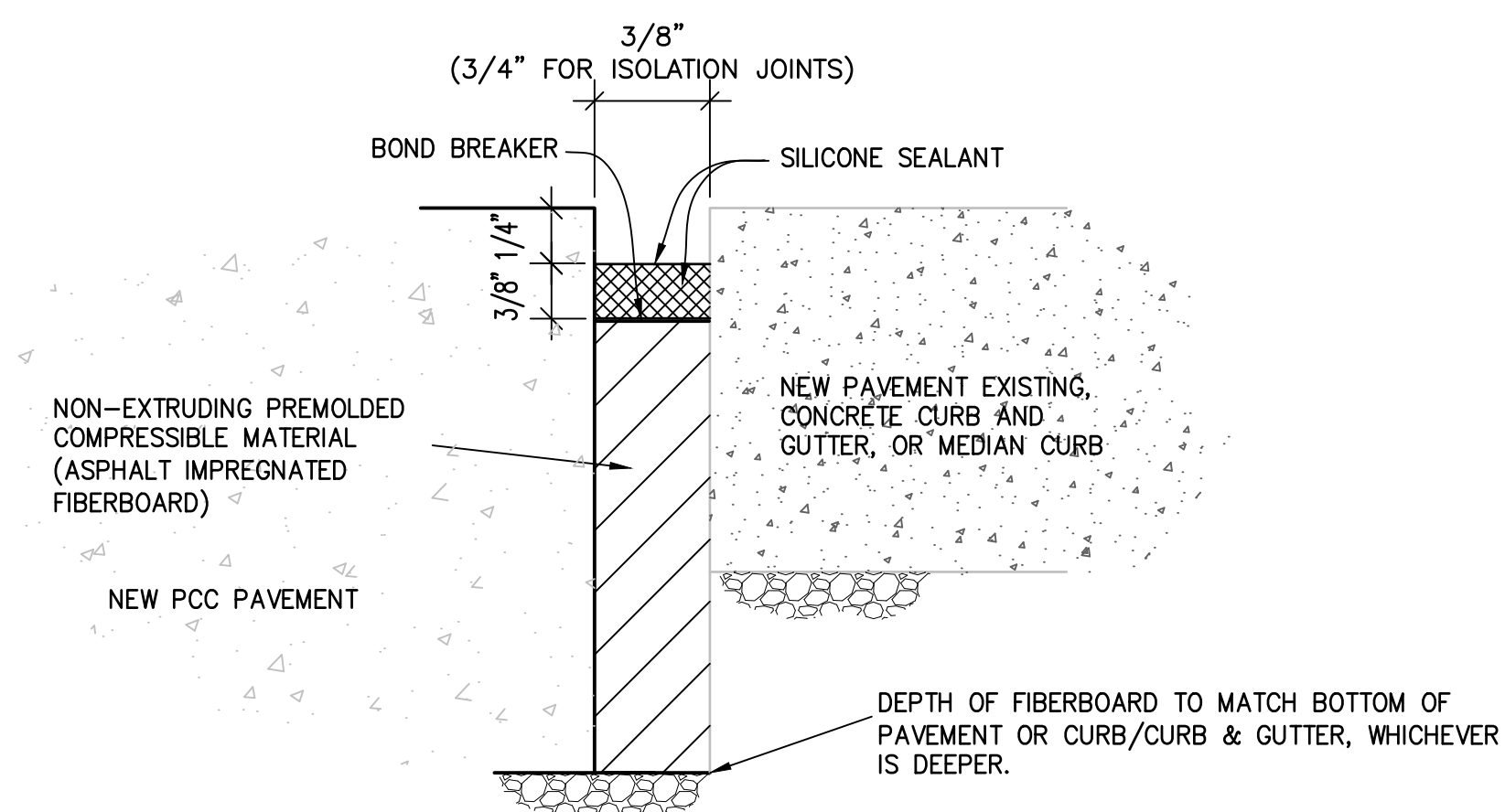


1 COMMERCIAL DRIVEWAY
DT-1 NTS

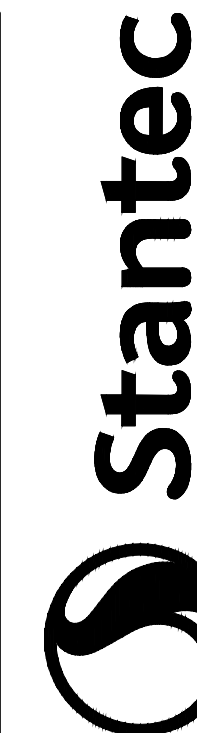
2 CATCH BASIN TYPE 3-R DRAIN INLET
DT-1 NTS



3 CATCH BASIN REINFORCEMENT DETAIL
DT-1



4 TYPICAL EXPANSION JOINT
DT-1



695 Sierra Center Parkway
Reno, Nevada, 89511
www.stantec.com

The Contractor shall verify and be responsible for all dimensions. DO NOT scale the drawing - any errors or omissions shall be reported to Stantec, without delay. The Copyright to all designs and drawings are the property of Stantec. Reproduction or use for any purpose other than that authorized by Stantec is forbidden.

Revision	By	Appr.	Issued
		YY/MM/DD	YY/MM/DD
		Appr.	Appr.

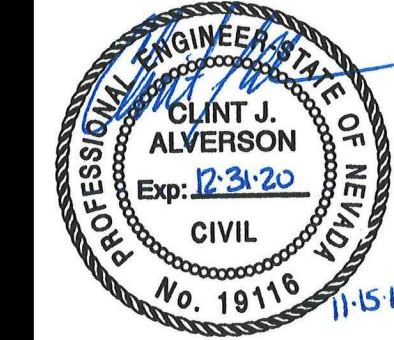
ISSUED FOR BID

Client/Project
CITY OF SPARKS

2020 PCC ALLEY PROJECT - ALLEY WAY "B"

Sparks, NV
Title
DETAILS

Permit-Seal



Project Number: T80101424
File Name: 01424_SA-8_DT.dwg

CJA	JAM	CJA	19.11.15
Dwn.	Chkd.	Dsgn.	YY/MM/DD

Drawing No. DT-1
Revision Sheet

