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CITY OF SPARKS

GENO MARTINI MAYOR

DONALD ABBOTT COUNCIL MEMBER WARD 1
ED LAWSON COUNCIL MEMBER WARD 2
RON SMITH COUNCIL MEMBER WARD 3
CHARLENE BYBEE COUNCIL MEMBER WARD 4
KRISTOPHER DAHIR COUNCIL MEMBER WARD 5
STEPHEN DRISCOLL CITY MANAGER



City of Sparks Community Services

APPROVALS:

JON ERICSON, P.E., P.T.O.F.

CITY ENGINEER

8/2

PLANS PREPARED AND SUBMITTED BY:



CLINT J. ALVERSON, P.E. PROJECT MANAGER

DATE

ENGINEER:

Reno, NV 8951 I

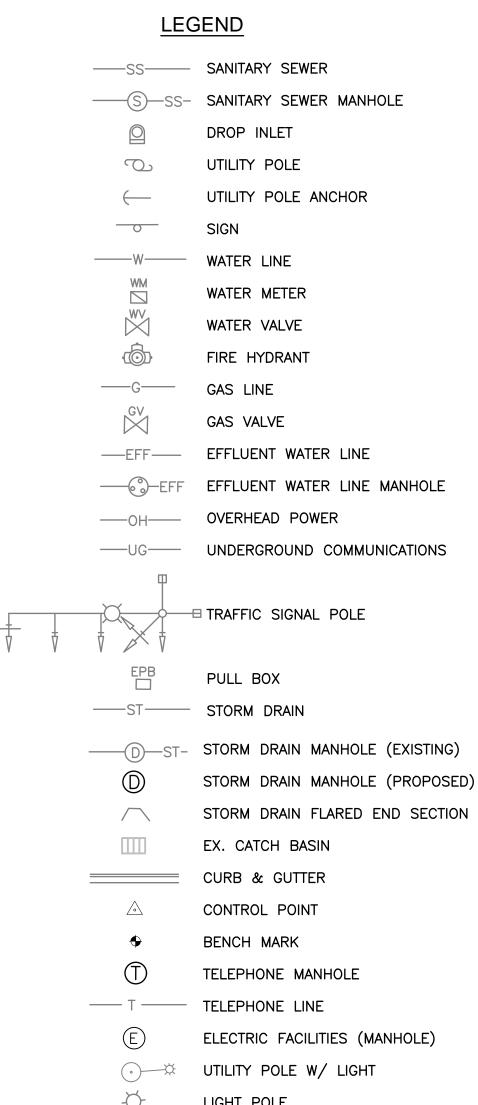
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GENERAL NOTES

- 1. LOCATIONS OF UNDERGROUND FACILITIES SHOWN ON THE PLANS ARE APPROXIMATE, AND WERE NOT DETERMINED BY FIELD INVESTIGATION. EXISTING UTILITIES ARE SHOWN BASED UPON AVAILABLE RECORD DRAWINGS. ALL UNDERGROUND UTILITIES MAY NOT BE SHOWN. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE ALL EXISTING UTILITY STRUCTURES, WHETHER SHOWN OR NOT. AND TO NOTIFY ALL UTILITY COMPANIES TO VERIFY IN THE FIELD THE LOCATION OF THEIR INSTALLATIONS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES FROM DAMAGE. THE EXPENSE OF REPAIR OR REPLACEMENT SHALL BE BORNE SOLELY BY THE CONTRACTOR. THE CONTRACTOR SHALL REQUEST FIELD MARKING OF EXISTING UTILITIES AT LEAST 48 HOURS IN ADVANCE OF BEGINNING CONSTRUCTION BY CALLING UNDERGROUND SERVICE ALERT AT 811. IT WILL BE THE CONTRACTORS RESPONSIBILITY TO MAINTAIN AND PROTECT ALL UTILITIES DURING CONSTRUCTION.
- 2. TOPOGRAPHIC INFORMATION CONTAINED WITHIN THESE CONSTRUCTION DOCUMENTS WAS PREPARED BY CONVENTIONAL FIELD TOPOGRAPHIC SURVEYS.
- THE BASIS OF BEARING IS GRID NORTH, NAD 83 (2011) NEVADA STATE PLANE COORDINATE SYSTEM, WEST ZONE, UTILIZING A COMBINED GRID TO GROUND FACTOR OF 1.000197939
- THE BASIS OF ELEVATION IS NAVD '88, BASED UPON CITY OF SPARKS BENCH MARK No. 59 ELEVATION = 4422.01
- 3. WORK IN PUBLIC STREETS, ONCE BEGUN, SHALL BE EXECUTED TO COMPLETION WITHOUT DELAY SO AS TO PROVIDE MINIMUM INCONVENIENCE TO ADJACENT PROPERTY OWNERS AND TO THE TRAVELING PUBLIC. THE CONSTRUCTION OF THE STREET IMPROVEMENTS SHALL ALLOW FOR THE PERPETUATION OF ALL EXISTING LEGAL ACCESSES AND EXISTING DRIVEWAYS, UNLESS OTHERWISE NOTED.
- 4. THE CONTRACTOR SHALL COOPERATE WITH OTHER CONTRACTORS OR UTILITY COMPANY FORCES WORKING ON THE SITE, AND WITH BUSINESS OWNERS ACTIVE OPERATIONS.
- 5. ALL SURFACES SHALL BE RESTORED TO THEIR ORIGINAL OR BETTER CONDITION AT THE COMPLETION OF CONSTRUCTION. EXISTING CONCRETE SUCH AS SIDEWALK, CURB, AND GUTTER SHALL BE REMOVED TO LIMITS MARKED IN FIELD BY THE ENGINEER. ALL REMOVAL MATERIALS SHALL BE DISPOSED OF OFF SITE AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL EXERCISE CAUTION WHEN WORKING ON PRIVATE PROPERTY.
- 6. AT LOCATIONS WHERE NEW UNDERGROUND FACILITIES CROSS EXISTING FACILITIES THE CONTRACTOR SHALL EXPOSE THE EXISTING FACILITY AND VERIFY THAT SUFFICIENT HORIZONTAL AND VERTICAL CLEARANCE EXISTS FOR THE NEW FACILITY TO BE CONSTRUCTED IN SUBSTANTIAL COMPLIANCE WITH THE PLANS. AT LOCATIONS WHERE NEW UNDERGROUND FACILITIES ARE TO BE CONNECTED TO EXISTING FACILITIES THE CONTRACTOR SHALL EXPOSE THE EXISTING FACILITY AND VERIFY THAT THE CONNECTION CAN BE MADE AS SHOWN ON THE PLANS. THIS VERIFICATION SHALL BE PERFORMED PRIOR TO ANY CONSTRUCTION. ANY CONFLICTS SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AS SOON AS THEY ARE DISCOVERED.
- 7. ALL DIMENSIONS TO CURBS OR CURB AND GUTTERS ARE TO THE FRONT FACE OF CURB UNLESS NOTED OTHERWISE ON THE DRAWINGS.
- 8. EXISTING DRAINAGE FACILITIES, OR INTERIM ENGINEER APPROVED ALTERNATIVES, SHALL BE KEPT IN SERVICE AT ALL TIMES DURING CONSTRUCTION. CONTRACTOR SHALL COMPLY WITH ALL REQUIREMENTS OF THE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) OF THE SUPPLEMENTAL GENERAL PROVISIONS OF THE SOLICITATION DOCUMENTS FOR SPARKS ALLEY WAY IMPROVEMENTS.
- 9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING ROADS, BUILDINGS OR OTHER STRUCTURES RESULTING FROM HIS CONSTRUCTION ACTIVITIES. REPAIRS SHALL BE MADE TO THE SATISFACTION OF THE CITY OF SPARKS, THE PROPERTY OWNERS, AND THE ENGINEER AT NO ADDITIONAL COST.
- 10. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF DISCREPANCIES BETWEEN THE INFORMATION SHOWN ON THESE DRAWINGS AND THE CONDITIONS EXISTING IN THE FIELD. THE CONTRACTOR SHALL COMPARE ALL DRAWINGS AND VERIFY THE FIGURES BEFORE STARTING THE WORK AND WILL BE RESPONSIBLE FOR ANY ERRORS WHICH MIGHT HAVE BEEN AVOIDED THEREBY. IF THE CONTRACTOR FAILS TO NOTIFY THE OWNER OR THEIR REPRESENTATIVE IN A TIMELY MANNER OF ANY APPARENT ERROR OR OMISSION ON THE PLANS OR SPECIFICATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING WORK INCORRECTLY DONE AT THE CONTRACTOR'S
- 11. THE USE OF POTABLE WATER FROM THE PUBLIC WATER SYSTEM FOR CONSTRUCTION PURPOSES IS PROHIBITED. CONSTRUCTION WATER USED FOR COMPACTION AND DUST CONTROL SHALL BE OBTAINED FROM THE RENO-SPARKS SEWAGE TREATMENT PLANT AT 8500 CLEAN WATER WAY, RENO NEVADA, TMWA'S TRUCK FILL STATIONS, OR ANOTHER APPROVED SOURCE.
- 12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT ALL MANHOLE RIMS AND ANY EXISTING UTILITY COVERS WITHIN THE CONSTRUCTION LIMITS ARE SET FLUSH WITH THE NEW FINISH
- 13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING STAGING AREA LOCATIONS. CONTRACTOR SHALL OBTAIN WRITTEN APPROVAL PRIOR TO USING A STAGING AREA. THE CONTRACTOR SHALL OBTAIN ANY PERMITS FROM THE CITY OF SPARKS THAT ARE REQUIRED FOR STOCKPILING/PROCESSING MATERIALS.
- 14. PAYMENT FOR WORK SHOWN ON THESE PLANS EITHER SPECIFIED OR INFERRED, BUT NOT INCLUDED IN THE BID PROPOSAL, SHALL BE CONSIDERED AS INCLUDED IN THE PRICE PAID FOR OTHER ITEMS OF WORK.
- 15. DURING THE ENTIRE DURATION OF THIS CONSTRUCTION CONTRACT, THE CONTRACTOR SHALL IMPLEMENT STRINGENT DUST CONTROL MEASURES IN ACCORDANCE WITH THE TERMS OF THE APPROVED DUST CONTROL PERMIT AND WASHOE COUNTY HEALTH DEPARTMENT RULES AND REGULATIONS. THE CONTRACTOR IS REQUIRED TO SUPPRESS DUST AT ALL TIMES, 24 HOURS A DAY, SEVEN (7) DAYS A WEEK, REGARDLESS OF WHEN CONSTRUCTION ACTIVITIES ARE OCCURRING.
- 16. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRS TO EXISTING LANDSCAPING DAMAGED BY OR THROUGH CONSTRUCTION ACTIVITIES. REPAIRS SHALL BE MADE TO THE SATISFACTION OF THE ENGINEER AND OWNER. THERE WILL BE NO DIRECT PAYMENT FOR THIS WORK.
- 17. CITY OF SPARKS STANDARD DETAILS SHALL APPLY EXCEPT WHERE OTHERWISE NOTED ON THE PLANS.
- 18. BEFORE ANY WORK IS STARTED IN THE STREET RIGHT-OF-WAY, THE CONTRACTOR SHALL INSTALL ADVANCED WARNING SIGNS FOR THE CONSTRUCTION ZONE. ALL CONSTRUCTION SIGNING, BARRICADING, AND TRAFFIC DELINEATION SHALL CONFORM TO THE "NEVADA DEPARTMENT OF TRANSPORTATION STANDARD PLANS FOR ROAD AND BRIDGE CONSTRUCTION" — CURRENT EDITION AND TO THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" - CURRENT EDITION AND BE APPROVED BY THE CITY OF SPARKS.
- 19. PROTECTION AND REPLACEMENT OF ALL SURVEY MONUMENTS OR PROPERTY STAKES NOT DELINEATED ON THE CONTRACT DRAWINGS SHALL BE THE CONTRACTOR'S RESPONSIBILITY. DAMAGED OR REMOVED MONUMENTS AND/OR PROPERTY STAKES SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.



LIGHT POLE

OOOO GUARDRAIL FENCE

BOLLARD --- PROPERTY LINE

— — CENTERLINE RIGHT OF WAY

— · · · — FLOWLINE

GRADE BREAK

NOTE: ALL SYMBOLS OR ABBREVIATIONS MAY NOT BE USED ON PLANS

ABBREVIATIONS

AC	ASPHALTIC CEMENT	
A.D.	ALGEBRAIC DIFFERENCE	
AP APP	ANGLE POINT ASPHALT PAVEMENT PATH	
ARV	AIR RELEASE VALVE	
BC	BEGINNING OF CURVE	
BF	BOTTOM OF FOOTING	
BFC	BACK FACE OF CURB	
BVCS BW /BOV	BEGINNING OF VERTICAL CURVE STATION VBACK OF SIDEWALK	
CB	CATCH BASIN	
C or CL	CENTERLINE	
CMP.	CORRUGATED METAL PIPE	
CONC.	CONCRETE	
CONST. DI	CONSTRUCT DROP INLET	
D.I.P.	DUCTILE IRON PIPE	
EC	END OF CURVE	
ĒĞ.	EXISTING GROUND	
ELEV. EP	ELEVATION EDGE OF PAVEMENT	
EVCE	END OF VERTICAL CURVE ELEVATION	
ĒVČŠ	END OF VERTICAL CURVE STATION	
ĘX.	EXISTING	
(e)	EXISTING	
FF FFC	FINISH FLOOR FRONT FACE OF CURB	
FG	FINISH GRADE	
FH	FIRE HYDRANT	
FL	FLOWLINE	
FLG FT	FLANGED FEET	
FV	FLUSH VALVE	
G	GAS	
GB	GRADE BREAK	
HORZ. HW	HORIZONTAL HEAD WALL	
IE	INVERT ELEVATION	
Κ̈	RATE OF VERTICAL CURVATURE	
L	LENGTH	
ĻĄT.	LATERAL	
LF LP	LINEAL FEETLOW POINT	
LT.	LEFT	
М.D.D.		
MIN.	MINIMUM	
MJ MPOC		
PC		
P.C.C.	PORTLAND CEMENT CONCRETE	
PCC	POINT OF COMPOUND CURVATURE	
PI	POINT OF INTERSECTION	
POC POT		
PPB	PEDESTRIAN PUSH BUTTON	
PRC	POINT OF REVERSE CURVATURE	
PT	POINT OF TANGENCY	
PVC PVI		
R'	RADIUS	
RCP	REINFORCED CONCRETE PIPE	
REF.	REFERENCE	
RET. RP		
ŔŤ.	RIGHT	
R/W	RIGHT OF WAY	
S´	SLOPE	
SD	STORM DRAIN MANUELE	
SDMH	STORM DRAIN MANHOLE	
SF SS		
SSMH	SANITARY SEWER MANHOLE	
SSPWC	STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION	
STA. SUP	STATION SHARED USE PATH	
SW	SIDEWALK	
TC		
TDC	TOP OF DEPRESSED CURB	
TP	TOP OF PAVEMENT	
TYP.	TYPICAL	
VERT. V.C.	VERTICAL VERTICAL CURVE	
v.C. V.P.I.	VERTICAL CORVE	
W	WATER	
W	WALK	

INDEX OF SHEETS

SHEET NO.	SHEET TITLE	DESCRIPTION OF SHEET
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3 OF 8	SI-1	PROPOSED SECTIONS OF IMPROVEMENT
4 OF 8	SP-1	SITE PLAN
5 OF 8	HC-1	HORIZONTAL CONTROL
6 OF 8	PP-1	PLAN AND PROFILE - ALLEY "A"
7 OF 8	DT-1	DETAIL SHEET
8 OF 8	DT-2	DETAIL SHEET

2019

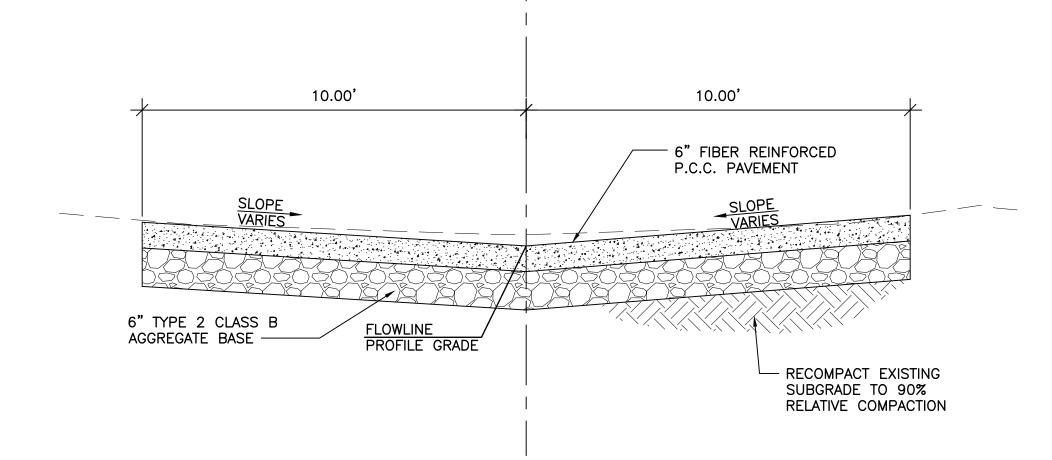


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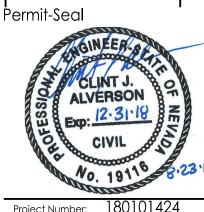
PROPOSED SECTION OF IMPROVEMENT - FLOWLINE AT CENTERLINE ALLEY "A" STA. 10+00.00 TO STA. 13+90.02

NOTES:

- 1. PCC PAVEMENT SHALL BE JOINTED AT 6'-8" ON CENTER FOR LONGITUDINAL JOINTS AND 8'-0" ON CENTER FOR TRANSVERSE JOINTS. JOINTS TO BE CUT A MINIMUM OF 4 HOURS TO A MAXIMUM OF 12 HOURS AFTER CONCLUSION OF BRUSH FINISHING.
- 2. SEE SPECIFICATIONS FOR MIX DESIGN.
- 3. AT STA. 10+00. INTERFACE WITH THE EXISTING DRIVEWAY SHALL BE DOWELED IF THE EXISTING DRIVEWAY THICKNESS IS 6" OR GREATER. SEE DETAIL 9/DT-2. NO DIRECT PAYMENT.

ALLEY "A" LIP/EP ELEVATIONS AND SLOPE							
LT. OFFSET	LT. ELEV	LT. SLOPE	STATION	RT. SLOPE	RT. ELEV	RT. OFFSET	
10.00	4422.49'	0.70%	10+25.00	3.40%	4422.76	10.00	
10.00	4422.15'	0.70%	10+50.00	4.20%	4422.50'	10.00	
10.00	4422.00'	0.50%	10+75.00	4.70%	4422.42'	10.00	
10.00	4421.90'	0.60%	11+00.00	3.10%	4422.05'	10.00	
10.00	4421.90'	1.40%	11+25.00	3.40%	4422.10'	10.00	
10.00	4421.86'	1.70%	11+50.00	3.30%	4422.02'	10.00	
10.00	4421.95'	3.40%	11+75.00	3.40%	4421.95'	10.00	
10.00	4421.86'	3.20%	12+00.00	4.60%	4422.00'	10.00	
10.00	4421.77	3.10%	12+25.00	3.60%	4421.82'	10.00	
10.00	4421.57	1.80%	12+50.00	2.00%	4421.59'	10.00	
10.00	4421.48'	1.70%	12+75.00	3.30%	4421.64'	10.00	
10.00	4421.57	2.10%	13+00.00	2.10%	4421.57	10.00	
10.00	4421.67	1.80%	13+25.00	1.80%	4421.67	10.00	
10.00	4421.77	1.60%	13+50.00	1.20%	4421.73	10.00	
10.00	4421.87	1.30%	13+75.00	1.10%	4421.85	10.00	

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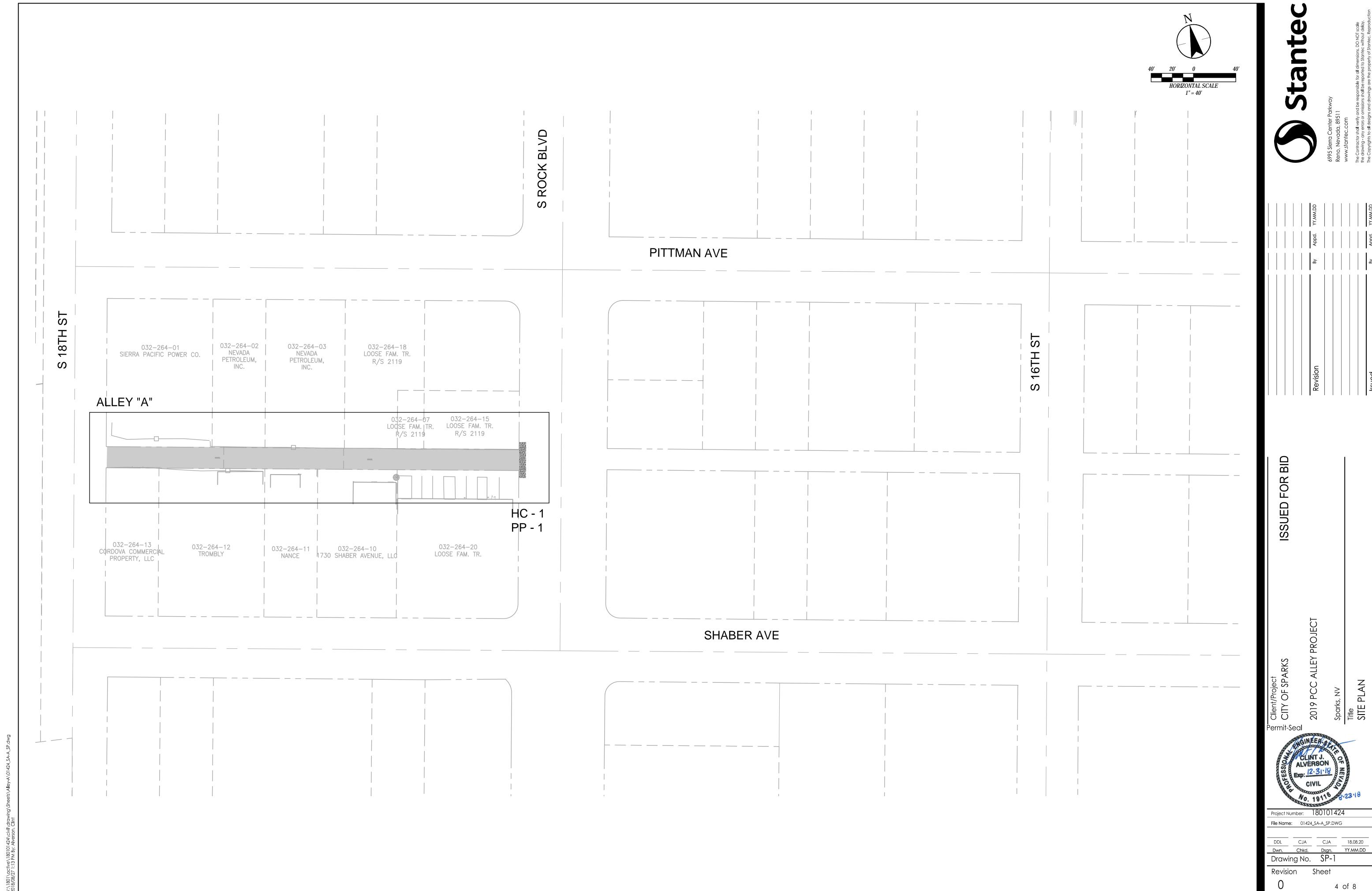


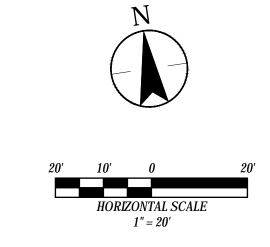
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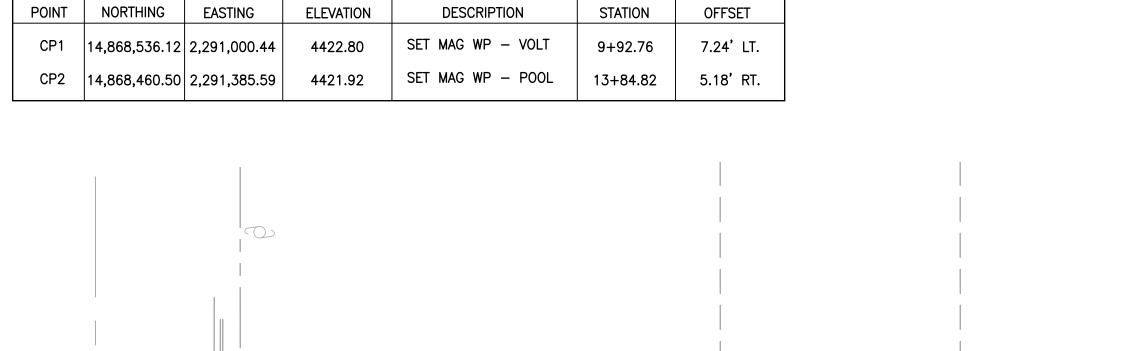
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Drawing No. SI-1

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SURVEY CONTROL POINT TABLE

STREET	032–264–01 SIERRA PACIFIC POWER CO.	-264-03 PETROLEUM, INC.	LOOSE FAM. TR.	032-264-07 DOSE FAM. TR. R/S 2119	032-264-15 LOOSE FAM. TR. R/S 2119	
18th	S68*29'57"W 20.86' 032-264-13	12+00		13+00		CP2————————————————————————————————————
SOUTH	QORDOVA COMMERCIAL PROPERTY, LLC TROM	/// ALLEY "A	032-264-10 1730 SHABER AVENUE, LLC		032-264-20 DOSE FAM. TR.	

2019 PCC /

Permit-Seal

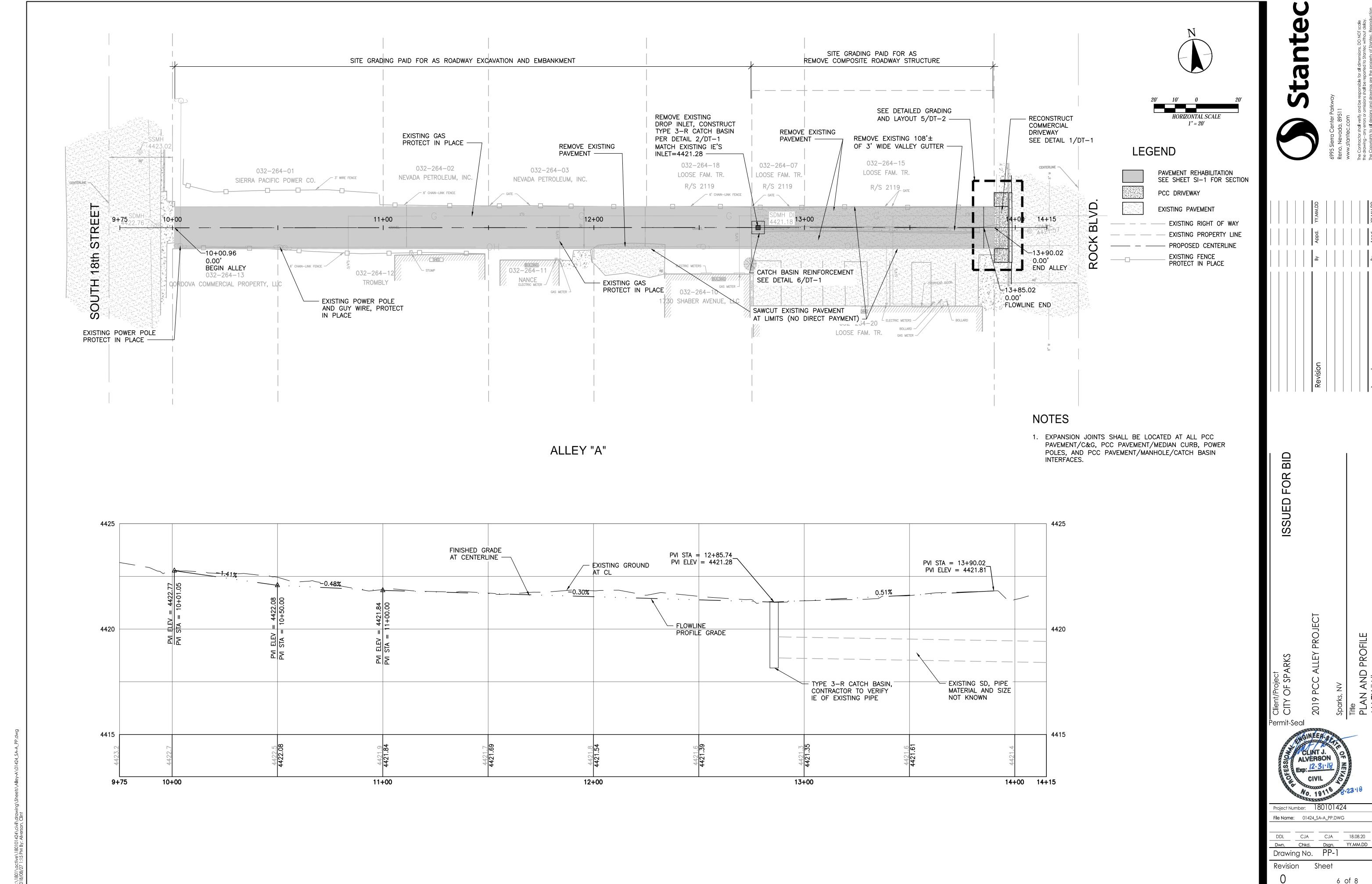
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Dwn. Chkd. Dsgn. YY.MM.DD

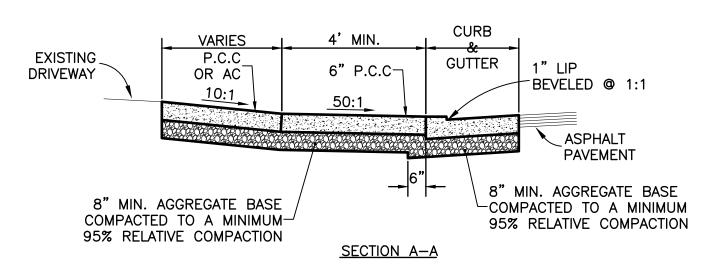
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Revision Sheet

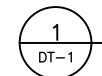
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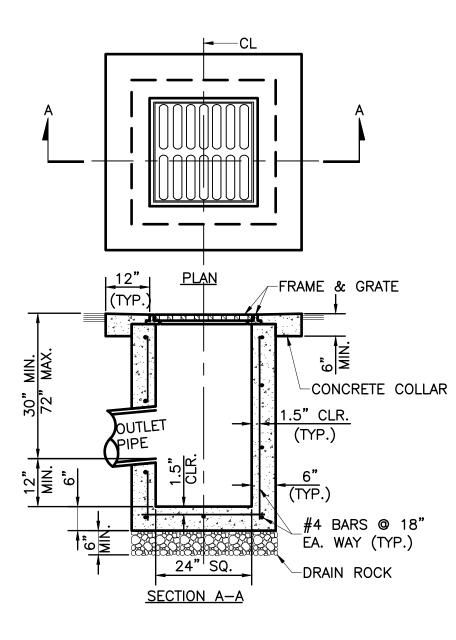
<u>PLAN</u>



- 1. SEE SPECIFICATIONS FOR MIX DESIGN.
- 2. AGGREGATE BASE MATERIAL UNDER DRIVEWAYS AND SIDEWALKS SHALL BE TYPE 2, CLASS B CRUSHED AGGREGATE BASE. ALL MATERIALS SHALL CONFORM TO SSPWC SECTION 200.
- 3. COMMERCIAL DRIVEWAYS MAY BE POURED MONOLITHIC WITH CURB AND GUTTER. COMMERCIAL DRIVEWAYS TO HAVE #4 BARS AT 18" ON CENTER LONGITUDINAL & TRANSVERSE EXTENDING INTO GUTTER PAN AND DRIVEWAY WINGS. MINIMUM 2" CONCRETE COVER FOR ALL REINFORCING BARS. WHEN COMMERCIAL DRIVEWAY APPROACH AND CURB & GUTTER IS POURED SEPARATELY, IT SHALL BE REQUIRED FOR EACH REINFORCING BAR TO BE DOWELED INTO ADJACENT CURB & GUTTER. DOWELS SHALL #4 REBAR, PENETRATE INTO CURB & GUTTER MINIMUM OF 6". SPACED AT 18" ON CENTER AND BE SECURELY TIED TO THE DRIVEWAY APPROACH REINFORCING.
- 4. IF JOINT EXISTS WITHIN 4 FEET OF DRIVEWAY, REMOVE SIDEWALK AND CURB AND GUTTER TO THAT JOINT.
- 5. ALL ADJACENT CONCRETE REMOVAL SHALL BE TO NEAT SAW CUT LINES AT RIGHT ANGLES. DOWEL INTO EXISTING ADJACENT CONCRETE DRIVEWAY APPROACH OR SIDEWALK WITH (2) No. 4 REINFORCEMENT BARS EQUALLY SPACED ACROSS WIDTH "W". DOWELS SHALL PENETRATE A MINIMUM OF 4" INTO EXISTING CONCRETE.
- 6. WEAKENED PLANE JOINTS SHALL BE CONSTRUCTED AT 5 FT INTERVALS AND IN ACCORDANCE WITH SECTION 312 OF THE SSPWC.



COMMERCIAL DRIVEWAY



NOTES:

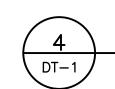
- 1. SEE SPECIFICATIONS FOR MIX DESIGN.
- 2. REINFORCING STEEL SHALL BE GRADE 40 AND HAVE 1.5" CLEAR COVER.
- 3. CONCRETE STRUCTURE MAY BE A PRE-CAST CONCRETE UNIT. BASE OF PRE-CAST CONCRETE UNIT SHALL BE PLACED ON 6" COMPACTED DRAIN ROCK.
- 4. FRAME & GRATE SHALL BE D&L I-9226 OR APPROVED EQUAL.
- 5. CATCH BASIN SHALL BE TRAFFIC-RATED AND USED ONLY AT LOW POINTS IN ALLEYS OR PARKING AREAS.
- 6. ALL CATCH BASINS, PUBLIC OR PRIVATE, SHALL BE PROVIDED WITH A "SUR-TRAP" OIL/WATER SEPARATOR OR APPROVED EQUAL.

CATCH BASIN TYPE 3-R DRAIN INLET

4" P.C.C CURB & SLOPE=50:1 SIDEWALK GUTTER 4" MIN. AGGREGATE BASE COMPACTED TO 95% 6" MIN. AGGREGATE BASE RELATIVE COMPACTION -COMPACTED TO A MINIMUM 95% RELATIVE COMPACTION SECTION A-A WEAKENED PLANE **JOINTS** <u>PLAN</u> SEE NOTE 5

- 1. SEE SPECIFICATIONS FOR MIX DESIGN.
- 2. AGGREGATE BASE MATERIAL UNDER SIDEWALKS SHALL BE TYPE 2, CLASS B CRUSHED AGGREGATE BASE. MATERIALS SHALL CONFORM TO SSPWC SECTION 200.
- 3. SIDEWALK WIDTH "W" SHALL BE 4 FT MIN. ON RESIDENTIAL STREETS AND 6 FT MIN. ON COLLECTOR AND ARTERIAL STREETS.
- 4. WEAKENED PLANE JOINTS SHALL BE CONSTRUCTED AT 5 FT INTERVALS AND ACCORDANCE WITH SECTION 312 OF THE SSPWC.
- 5. ALL ADJACENT CONCRETE REMOVAL SHALL BE TO NEAT SAW CUT LINES AT RIGHT ANGLES TO NEW SIDEWALK. DOWEL INTO EXISTING ADJACENT CONCRETE SIDEWALK WITH A MINIMUM OF TWO (2) No. 4 REINFORCEMENT BARS EQUALLY SPACED ACROSS WIDTH "W". DOWELS SHALL PENETRATE A MINIMUM OF 4" INTO EXISTING CONCRETE.
- 6. SIDEWALKS SHALL NOT BE POURED MONOLITHICALLY WITH CURBS.
- 7. COLORED CONCRETE AND PAVERS ARE NOT ALLOWED.
- 8. TUNNELING AND/OR BORING IS NOT ALLOWED.

SIDEWALK



NOTES:

- 1. SEE SPECIFICATIONS FOR MIX DESIGN.
- 2. AGGREGATE BASE MATERIAL UNDER AND BEHIND CURB AND GUTTER SHALL BE TYPE 2, CLASS B CRUSHED AGGREGATE BASE. MATERIALS SHALL CONFORM TO SSPWC SECTION

TYPE 1

6" MIN. AGGREGATE BASE -COMPACTED TO A MINIMUM 95% RELATIVE COMPACTION

- 3. WEAKENED PLANE JOINTS SHALL BE EVERY 10 FEET AND LOCATED ON THE BACK, TOP AND FACE OF THE CURB AND THE TOP OF THE GUTTER PAN.
- 4. CURB & GUTTER SECTIONS SHALL BE PLACED SEPARATELY FROM SIDEWALK SECTIONS. WHEN SIDEWALK IS NOT REQUIRED DIRECTLY BEHIND THE CURB, BACKFILL TO TOP OF CURB FOR A HORIZONTAL DISTANCE OF 12" FROM BACK FACE OF CURB AND COMPACT TO 90% RELATIVE COMPACTION.
- 5. FOR REPLACEMENT OF EXISTING CURB AND GUTTER, MATCH EXISTING TYPE.

CURB AND GUTTER

2019

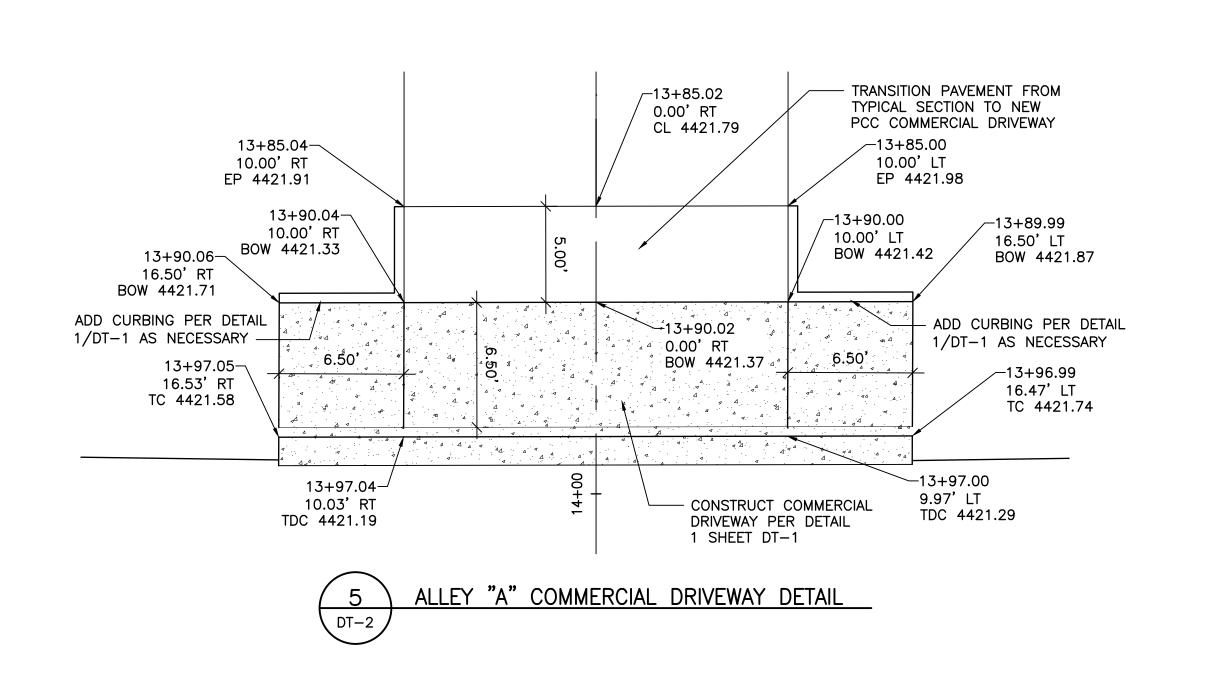
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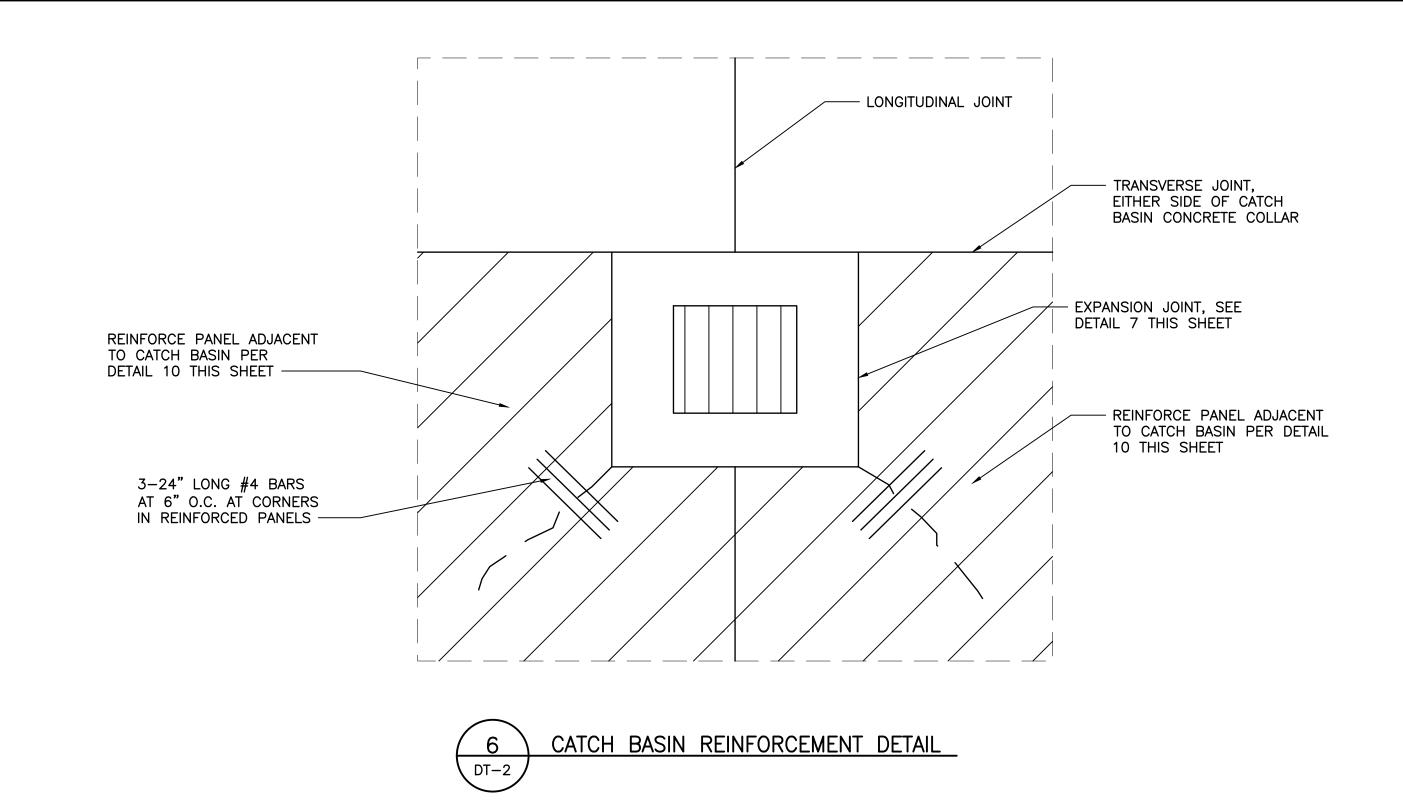
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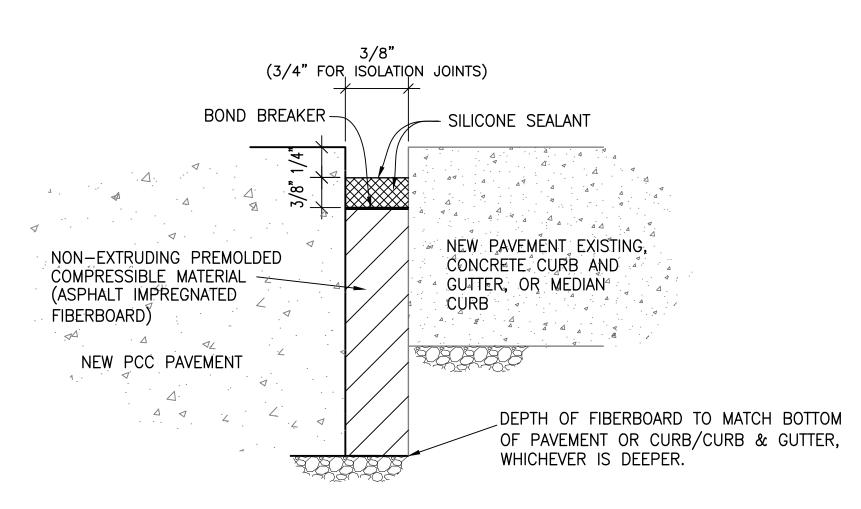
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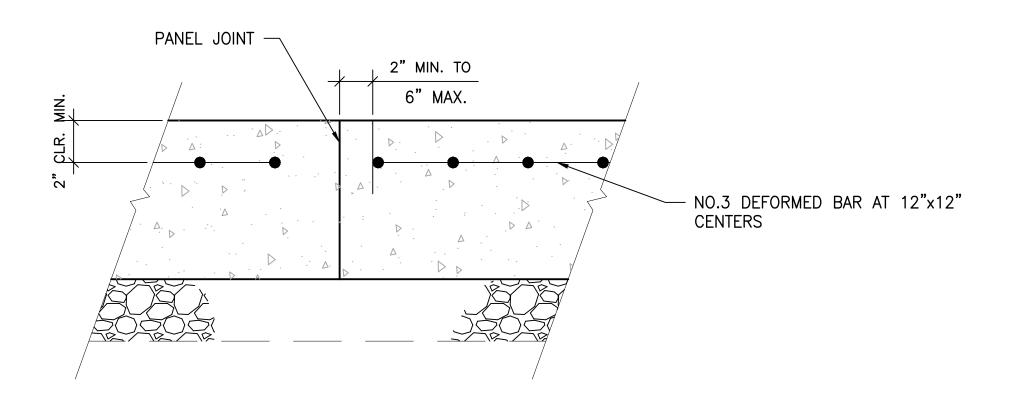
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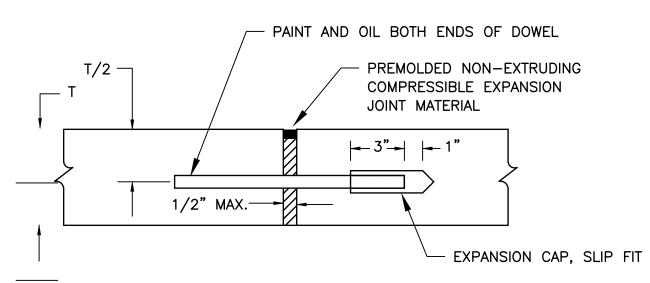






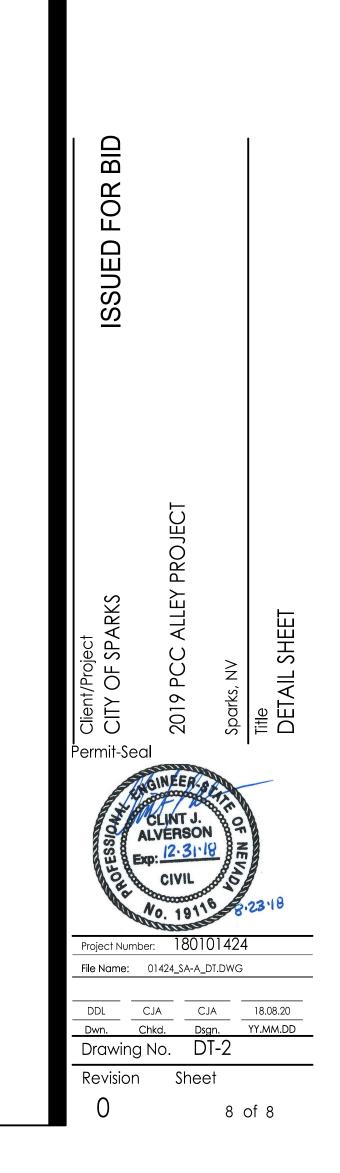






- 1. DOWEL BAR SPACING SHALL BE 15".
- 2. DOWEL BARS SHALL NOT BE PLACED WITHIN 15" OF LONGITUDINAL JOINTS.





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