

# A CITY OF SPARKS PROJECT

## CITY OF SPARKS PROJECT BID NO. 18/19-008

### 2019 PCB ALLEY PROJECT

### SPARKS, WASHOE COUNTY, NEVADA

CITY OF SPARKS

GENO MARTINI	MAYOR
DONALD ABBOTT	COUNCIL MEMBER WARD 1
ED LAWSON	COUNCIL MEMBER WARD 2
RON SMITH	COUNCIL MEMBER WARD 3
CHARLENE BYBEE	COUNCIL MEMBER WARD 4
KRISTOPHER DAHIR	COUNCIL MEMBER WARD 5
STEPHEN DRISCOLL	CITY MANAGER



APPROVALS:

*[Signature]* 8/28/18  
 JON ERICSON, P.E., P.T.O.E.  
 CITY ENGINEER DATE

PLANS PREPARED AND SUBMITTED BY:



*[Signature]*  
 CLINT J. ALVERSON, P.E.  
 PROJECT MANAGER DATE

ENGINEER:



6995 Sierra Center Parkway  
Reno, NV 89511  
www.stantec.com



STANTEC PROJECT NO. 180101424

C-0









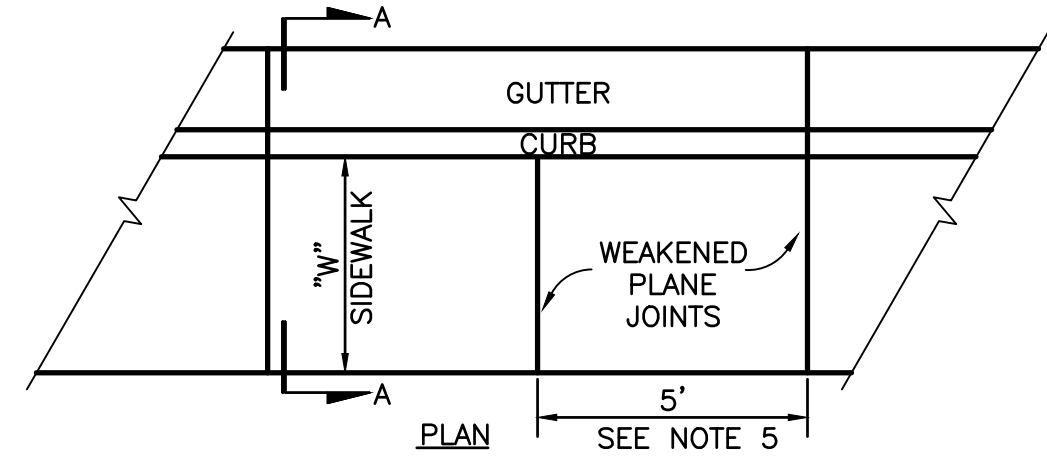
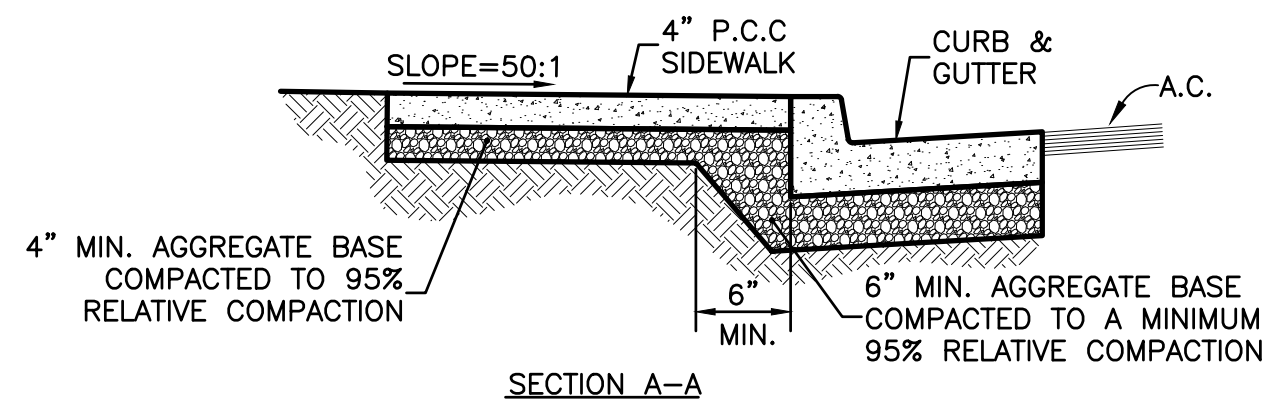
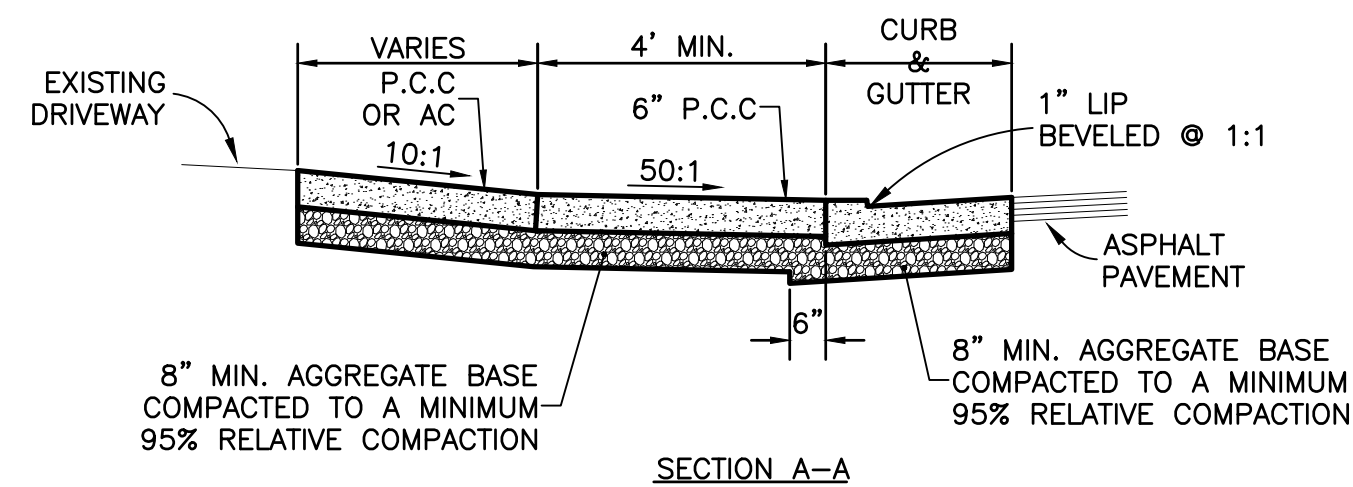
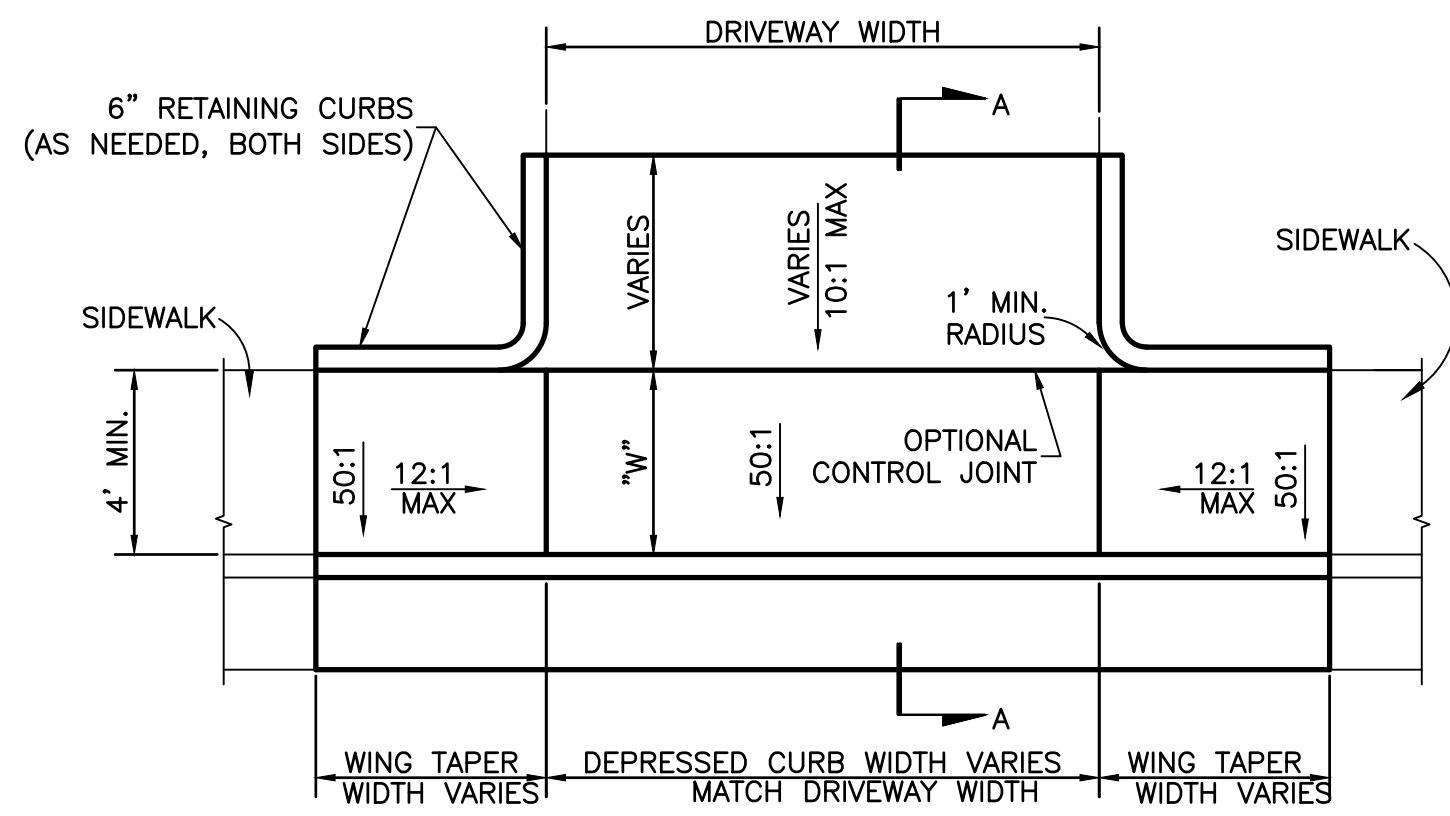












**NOTES:**

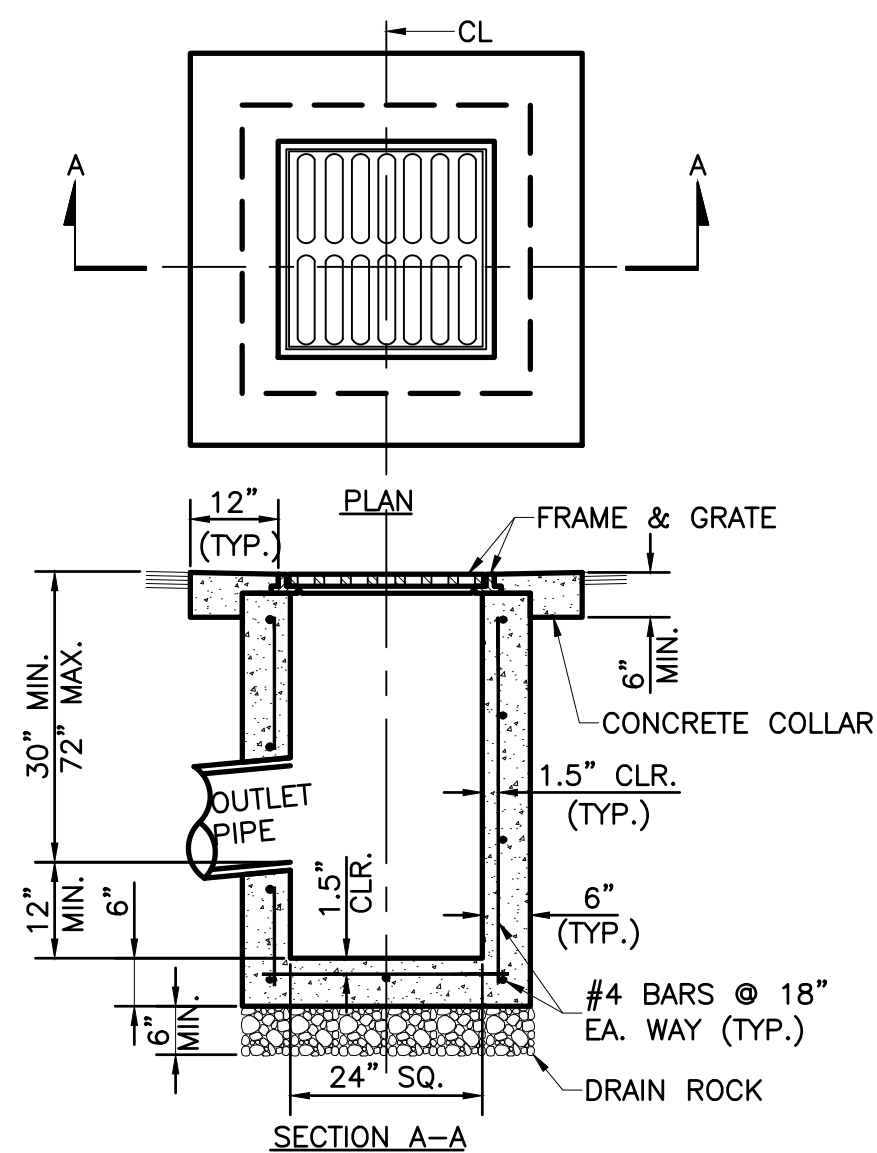
1. SEE SPECIFICATIONS FOR MIX DESIGN.
2. AGGREGATE BASE MATERIAL UNDER SIDEWALKS SHALL BE TYPE 2, CLASS B CRUSHED AGGREGATE BASE. MATERIALS SHALL CONFORM TO SSPWC SECTION 200.
3. SIDEWALK WIDTH "W" SHALL BE 4 FT MIN. ON RESIDENTIAL STREETS AND 6 FT MIN. ON COLLECTOR AND ARTERIAL STREETS.
4. WEAKENED PLANE JOINTS SHALL BE CONSTRUCTED AT 5 FT INTERVALS AND ACCORDANCE WITH SECTION 312 OF THE SSPWC.
5. ALL ADJACENT CONCRETE REMOVAL SHALL BE TO NEAT SAW CUT LINES AT RIGHT ANGLES TO NEW SIDEWALK. DOWEL INTO EXISTING ADJACENT CONCRETE SIDEWALK WITH A MINIMUM OF TWO (2) No. 4 REINFORCEMENT BARS EQUALLY SPACED ACROSS WIDTH "W". DOWELS SHALL PENETRATE A MINIMUM OF 4" INTO EXISTING CONCRETE.
6. SIDEWALKS SHALL NOT BE POURED MONOLITHICALLY WITH CURBS.
7. COLORED CONCRETE AND PAVERS ARE NOT ALLOWED.
8. TUNNELING AND/OR BORING IS NOT ALLOWED.

**3** SIDEWALK  
DT-1 NTS

**NOTES:**

1. SEE SPECIFICATIONS FOR MIX DESIGN.
2. AGGREGATE BASE MATERIAL UNDER DRIVEWAYS AND SIDEWALKS SHALL BE TYPE 2, CLASS B CRUSHED AGGREGATE BASE. ALL MATERIALS SHALL CONFORM TO SSPWC SECTION 200.
3. COMMERCIAL DRIVEWAYS MAY BE POURED MONOLITHIC WITH CURB AND GUTTER. COMMERCIAL DRIVEWAYS TO HAVE #4 BARS AT 18" ON CENTER LONGITUDINAL & TRANSVERSE EXTENDING INTO GUTTER PAN AND DRIVEWAY WINGS. MINIMUM 2" CONCRETE COVER FOR ALL REINFORCING BARS. WHEN COMMERCIAL DRIVEWAY APPROACH AND CURB & GUTTER IS POURED SEPARATELY, IT SHALL BE REQUIRED FOR EACH REINFORCING BAR TO BE DOWELED INTO ADJACENT CURB & GUTTER. DOWELS SHALL #4 REBAR, PENETRATE INTO CURB & GUTTER MINIMUM OF 6", SPACED AT 18" ON CENTER AND BE SECURELY TIED TO THE DRIVEWAY APPROACH REINFORCING.
4. IF JOINT EXISTS WITHIN 4 FEET OF DRIVEWAY, REMOVE SIDEWALK AND CURB AND GUTTER TO THAT JOINT.
5. ALL ADJACENT CONCRETE REMOVAL SHALL BE TO NEAT SAW CUT LINES AT RIGHT ANGLES. DOWEL INTO EXISTING ADJACENT CONCRETE DRIVEWAY APPROACH OR SIDEWALK WITH (2) No. 4 REINFORCEMENT BARS EQUALLY SPACED ACROSS WIDTH "W". DOWELS SHALL PENETRATE A MINIMUM OF 4" INTO EXISTING CONCRETE.
6. WEAKENED PLANE JOINTS SHALL BE CONSTRUCTED AT 5 FT INTERVALS AND IN ACCORDANCE WITH SECTION 312 OF THE SSPWC.

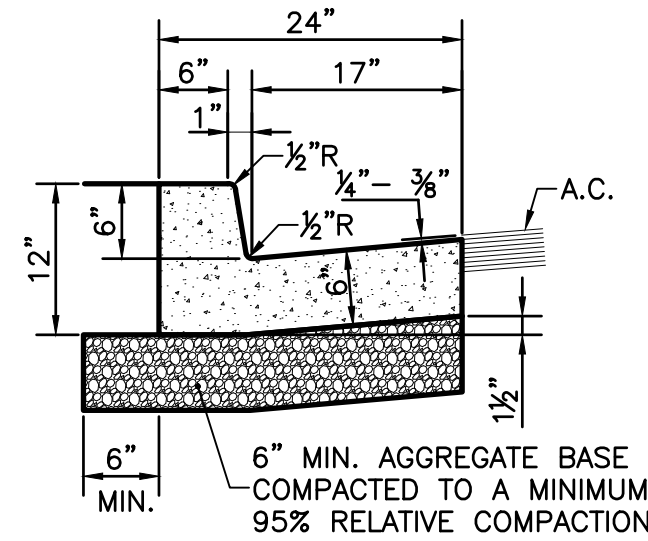
**1** COMMERCIAL DRIVEWAY  
DT-1 NTS



**NOTES:**

1. SEE SPECIFICATIONS FOR MIX DESIGN.
2. REINFORCING STEEL SHALL BE GRADE 40 AND HAVE 1.5" CLEAR COVER.
3. CONCRETE STRUCTURE MAY BE A PRE-CAST CONCRETE UNIT. BASE OF PRE-CAST CONCRETE UNIT SHALL BE PLACED ON 6" COMPACTED DRAIN ROCK.
4. FRAME & GRATE SHALL BE D&L I-9226 OR APPROVED EQUAL.
5. CATCH BASIN SHALL BE TRAFFIC-RATED AND USED ONLY AT LOW POINTS IN ALLEYS OR PARKING AREAS.
6. ALL CATCH BASINS, PUBLIC OR PRIVATE, SHALL BE PROVIDED WITH A "SUR-TRAP" OIL/WATER SEPARATOR OR APPROVED EQUAL.

**2** CATCH BASIN TYPE 3-R DRAIN INLET  
DT-1 NTS

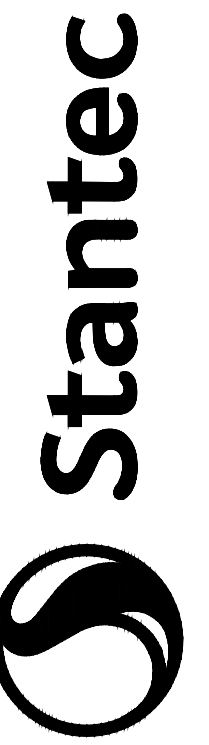


TYPE 1

**NOTES:**

1. SEE SPECIFICATIONS FOR MIX DESIGN.
2. AGGREGATE BASE MATERIAL UNDER AND BEHIND CURB AND GUTTER SHALL BE TYPE 2, CLASS B CRUSHED AGGREGATE BASE. MATERIALS SHALL CONFORM TO SSPWC SECTION 200.
3. WEAKENED PLANE JOINTS SHALL BE EVERY 10 FEET AND LOCATED ON THE BACK, TOP AND FACE OF THE CURB AND THE TOP OF THE GUTTER PAN.
4. CURB & GUTTER SECTIONS SHALL BE PLACED SEPARATELY FROM SIDEWALK SECTIONS. WHEN SIDEWALK IS NOT REQUIRED DIRECTLY BEHIND THE CURB, BACKFILL TO TOP OF CURB FOR A HORIZONTAL DISTANCE OF 12" FROM BACK FACE OF CURB AND COMPACT TO 90% RELATIVE COMPACTION.
5. FOR REPLACEMENT OF EXISTING CURB AND GUTTER, MATCH EXISTING TYPE.

**4** CURB AND GUTTER  
DT-1 NTS



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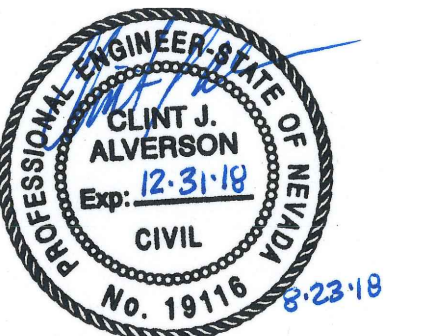
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Revision	By	Appd.	Y/M/MD	Issued	By	Appd.	Y/M/MD

ISSUED FOR BID

Client/Project  
CITY OF SPARKS  
2019 PCC ALLEY PROJECT

Permit-Seal



Project Number: 180101424  
File Name: 01424\_SA-A\_DT.DWG

DDL	CJA	CJA	18.08.20
Dwn.	Chk'd.	Dsgn.	YY/MM/DD

Drawing No. DT-1

Revision Sheet

