

A CITY OF SPARKS PROJECT

ISSUED FOR BID

2021 PCC ALLEYS

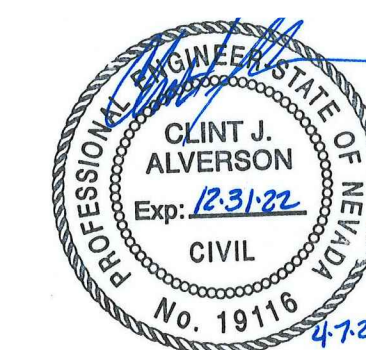
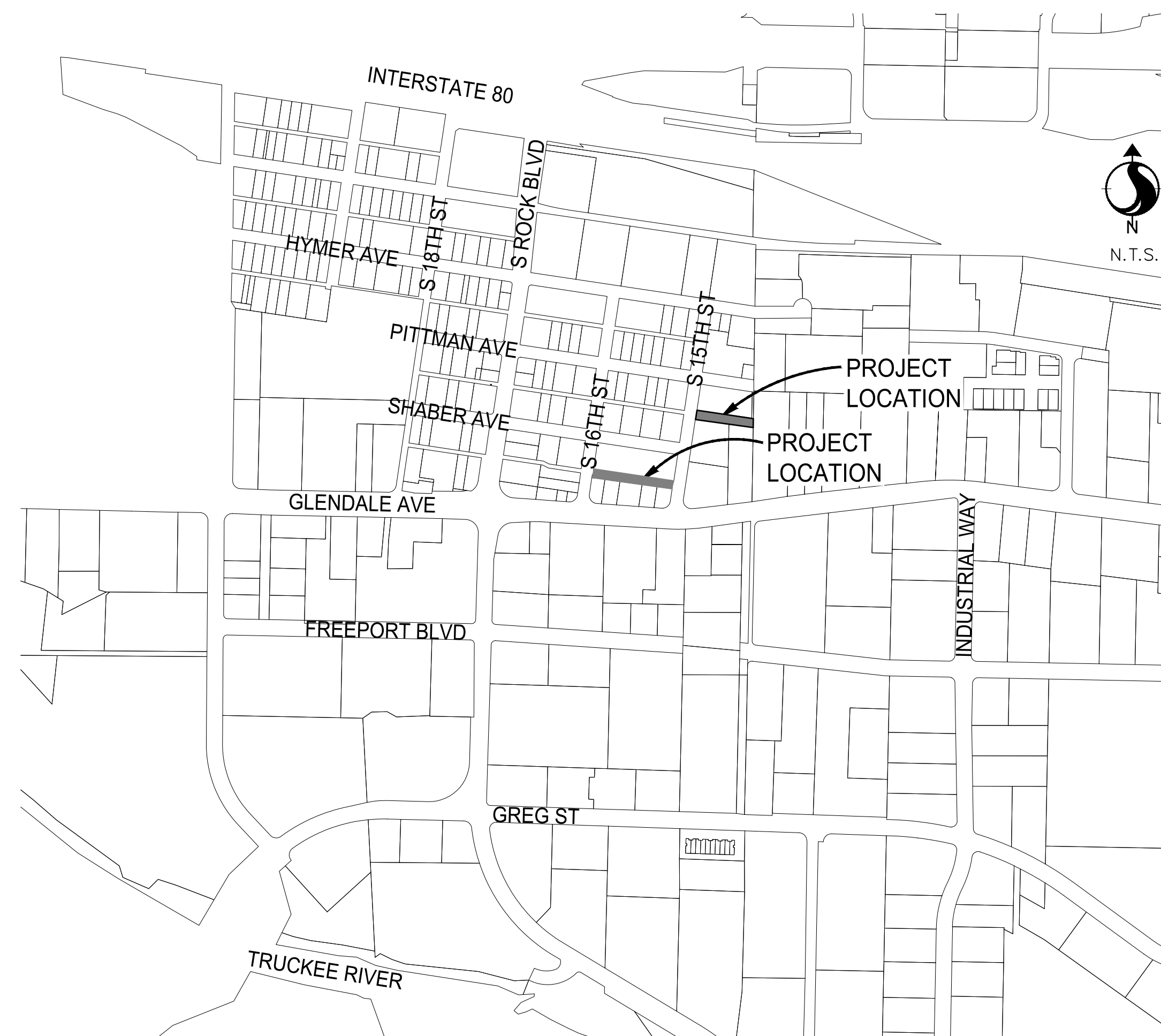
CITY OF SPARKS PROJECT BID NO. 20/21-017

PUBLIC WORKS PROJECT NO. WA-2021-255

SPARKS, WASHOE COUNTY, NEVADA

CITY OF SPARKS

ED LAWSON	MAYOR
DONALD ABBOTT	COUNCIL MEMBER WARD 1
DIAN VANDERWELL	COUNCIL MEMBER WARD 2
PAUL ANDERSON	COUNCIL MEMBER WARD 3
CHARLENE BYBEE	COUNCIL MEMBER WARD 4
KRISTOPHER DAHIR	COUNCIL MEMBER WARD 5
NEIL KRUTZ	CITY MANAGER



APPROVALS:

JON R. ERICSON, P.E., P.T.O.E. _____ DATE
CITY ENGINEER

PLANS PREPARED AND SUBMITTED BY:

Clint Alverson _____ DATE
CLINT J. ALVERSON, P.E.
PROJECT MANAGER

ENGINEER:



6995 Sierra Center Parkway
Reno, NV 89511
www.stantec.com



STANTEC PROJECT NO. 180101424

C-0

GENERAL NOTES

- LOCATIONS OF UNDERGROUND FACILITIES SHOWN ON THE PLANS ARE APPROXIMATE, AND WERE NOT DETERMINED BY FIELD INVESTIGATION. EXISTING UTILITIES ARE SHOWN BASED UPON AVAILABLE RECORD DRAWINGS. ALL UNDERGROUND UTILITIES MAY NOT BE SHOWN. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE ALL EXISTING UTILITY STRUCTURES, WHETHER SHOWN OR NOT, AND TO NOTIFY ALL UTILITY COMPANIES TO VERIFY IN THE FIELD THE LOCATION OF THEIR INSTALLATIONS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL PROTECT ALL UTILITY STRUCTURES FROM DAMAGE. THE EXPENSE OF REPAIR OR REPLACEMENT SHALL BE BORNE SOLELY BY THE CONTRACTOR. THE CONTRACTOR SHALL REQUEST FIELD MARKING OF EXISTING UTILITIES AT LEAST 48 HOURS IN ADVANCE OF BEGINNING CONSTRUCTION BY CALLING UNDERGROUND SERVICE ALERT AT 811. IT WILL BE THE CONTRACTORS RESPONSIBILITY TO MAINTAIN AND PROTECT ALL UTILITIES DURING CONSTRUCTION.
- TOPOGRAPHIC INFORMATION CONTAINED WITHIN THESE CONSTRUCTION DOCUMENTS WAS PREPARED BY CONVENTIONAL FIELD TOPOGRAPHIC SURVEYS.
THE BASIS OF BEARING IS GRID NORTH, NAD 83 (2011) NEVADA STATE PLANE COORDINATE SYSTEM, WEST ZONE, UTILIZING A COMBINED GRID TO GROUND FACTOR OF 1.000197939
THE BASIS OF ELEVATION IS NAVD '88, BASED UPON CITY OF SPARKS BENCH MARK No. 59 ELEVATION = 4422.01'
- WORK IN PUBLIC STREETS, ONCE BEGUN, SHALL BE EXECUTED TO COMPLETION WITHOUT DELAY SO AS TO PROVIDE MINIMUM INCONVENIENCE TO ADJACENT PROPERTY OWNERS AND TO THE TRAVELING PUBLIC. THE CONSTRUCTION OF THE STREET IMPROVEMENTS SHALL ALLOW FOR THE PERPETUATION OF ALL EXISTING LEGAL ACCESSSES AND EXISTING DRIVEWAYS, UNLESS OTHERWISE NOTED.
- THE CONTRACTOR SHALL COOPERATE WITH OTHER CONTRACTORS OR UTILITY COMPANY FORCES WORKING ON THE SITE, AND WITH BUSINESS OWNERS ACTIVE OPERATIONS.
- ALL SURFACES SHALL BE RESTORED TO THEIR ORIGINAL OR BETTER CONDITION AT THE COMPLETION OF CONSTRUCTION. EXISTING CONCRETE SUCH AS SIDEWALK, CURB, AND GUTTER SHALL BE REMOVED TO LIMITS MARKED IN FIELD BY THE ENGINEER. ALL REMOVAL MATERIALS SHALL BE DISPOSED OF OFF SITE AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL EXERCISE CAUTION WHEN WORKING ON PRIVATE PROPERTY.
- AT LOCATIONS WHERE NEW UNDERGROUND FACILITIES CROSS EXISTING FACILITIES THE CONTRACTOR SHALL EXPOSE THE EXISTING FACILITY AND VERIFY THAT SUFFICIENT HORIZONTAL AND VERTICAL CLEARANCE EXISTS FOR THE NEW FACILITY TO BE CONSTRUCTED IN SUBSTANTIAL COMPLIANCE WITH THE PLANS. AT LOCATIONS WHERE NEW UNDERGROUND FACILITIES ARE TO BE CONNECTED TO EXISTING FACILITIES THE CONTRACTOR SHALL EXPOSE THE EXISTING FACILITY AND VERIFY THAT THE CONNECTION CAN BE MADE AS SHOWN ON THE PLANS. THIS VERIFICATION SHALL BE PERFORMED PRIOR TO ANY CONSTRUCTION. ANY CONFLICTS SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION AS SOON AS THEY ARE DISCOVERED.
- ALL DIMENSIONS TO CURBS OR CURB AND GUTTERS ARE TO THE FRONT FACE OF CURB UNLESS NOTED OTHERWISE ON THE DRAWINGS.
- EXISTING DRAINAGE FACILITIES, OR INTERIM ENGINEER APPROVED ALTERNATIVES, SHALL BE KEPT IN SERVICE AT ALL TIMES DURING CONSTRUCTION. CONTRACTOR SHALL COMPLY WITH ALL REQUIREMENTS OF THE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) OF THE SUPPLEMENTAL GENERAL PROVISIONS OF THE SOLICITATION DOCUMENTS FOR 2021 PCC ALLEYS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING ROADS, BUILDINGS OR OTHER STRUCTURES RESULTING FROM HIS CONSTRUCTION ACTIVITIES. REPAIRS SHALL BE MADE TO THE SATISFACTION OF THE CITY OF SPARKS, THE PROPERTY OWNERS, AND THE ENGINEER AT NO ADDITIONAL COST.
- THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF DISCREPANCIES BETWEEN THE INFORMATION SHOWN ON THESE DRAWINGS AND THE CONDITIONS EXISTING IN THE FIELD. THE CONTRACTOR SHALL COMPARE ALL DRAWINGS AND VERIFY THE FIGURES BEFORE STARTING THE WORK AND WILL BE RESPONSIBLE FOR ANY ERRORS WHICH MIGHT HAVE BEEN AVOIDED THEREBY. IF THE CONTRACTOR FAILS TO NOTIFY THE OWNER OR THEIR REPRESENTATIVE IN A TIMELY MANNER OF ANY APPARENT ERROR OR OMISSION ON THE PLANS OR SPECIFICATION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING WORK INCORRECTLY DONE AT THE CONTRACTOR'S EXPENSE.
- THE USE OF POTABLE WATER FROM THE PUBLIC WATER SYSTEM FOR CONSTRUCTION PURPOSES IS PROHIBITED. CONSTRUCTION WATER USED FOR COMPACTION AND DUST CONTROL SHALL BE OBTAINED FROM THE RENO-SPARKS SEWAGE TREATMENT PLANT AT 8500 CLEAN WATER WAY, RENO NEVADA, TMWA'S TRUCK FILL STATIONS, OR ANOTHER APPROVED SOURCE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT ALL MANHOLE RIMS AND ANY EXISTING UTILITY COVERS WITHIN THE CONSTRUCTION LIMITS ARE SET FLUSH WITH THE NEW FINISH GRADE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING STAGING AREA LOCATIONS. CONTRACTOR SHALL OBTAIN WRITTEN APPROVAL PRIOR TO USING A STAGING AREA. THE CONTRACTOR SHALL OBTAIN ANY PERMITS FROM THE CITY OF SPARKS THAT ARE REQUIRED FOR STOCKPILING/PROCESSING MATERIALS.
- PAYMENT FOR WORK SHOWN ON THESE PLANS EITHER SPECIFIED OR INFERRED, BUT NOT INCLUDED IN THE BID PROPOSAL, SHALL BE CONSIDERED AS INCLUDED IN THE PRICE PAID FOR OTHER ITEMS OF WORK.
- DURING THE ENTIRE DURATION OF THIS CONSTRUCTION CONTRACT, THE CONTRACTOR SHALL IMPLEMENT STRINGENT DUST CONTROL MEASURES IN ACCORDANCE WITH THE TERMS OF THE APPROVED DUST CONTROL PERMIT AND WASHOE COUNTY HEALTH DEPARTMENT RULES AND REGULATIONS. THE CONTRACTOR IS REQUIRED TO SUPPRESS DUST AT ALL TIMES, 24 HOURS A DAY, SEVEN (7) DAYS A WEEK, REGARDLESS OF WHEN CONSTRUCTION ACTIVITIES ARE OCCURRING.
- THE CONTRACTOR IS RESPONSIBLE FOR REPAIRS TO EXISTING LANDSCAPING DAMAGED BY OR THROUGH CONSTRUCTION ACTIVITIES. REPAIRS SHALL BE MADE TO THE SATISFACTION OF THE ENGINEER AND OWNER. THERE WILL BE NO DIRECT PAYMENT FOR THIS WORK.
- CITY OF SPARKS STANDARD DETAILS SHALL APPLY EXCEPT WHERE OTHERWISE NOTED ON THE PLANS.
- BEFORE ANY WORK IS STARTED IN THE STREET RIGHT-OF-WAY, THE CONTRACTOR SHALL INSTALL ADVANCED WARNING SIGNS FOR THE CONSTRUCTION ZONE. ALL CONSTRUCTION SIGNING, BARRICADING, AND TRAFFIC DELINEATION SHALL CONFORM TO THE "NEVADA DEPARTMENT OF TRANSPORTATION STANDARD PLANS FOR ROAD AND BRIDGE CONSTRUCTION" - CURRENT EDITION AND TO THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" - CURRENT EDITION AND BE APPROVED BY THE CITY OF SPARKS.
- PROTECTION AND REPLACEMENT OF ALL SURVEY MONUMENTS OR PROPERTY STAKES NOT DELINEATED ON THE CONTRACT DRAWINGS SHALL BE THE CONTRACTOR'S RESPONSIBILITY. DAMAGED OR REMOVED MONUMENTS AND/OR PROPERTY STAKES SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- PROTECT IN PLACE EXISTING WASHOE COUNTY MONITORING WELL. SEE PLANS AND DETAILS FOR ADJUSTING WELL TO FINISHED GRADE.

LEGEND

- SANITARY SEWER
- SANITARY SEWER MANHOLE
- DROP INLET
- UTILITY POLE
- UTILITY POLE ANCHOR
- SIGN
- WATER LINE
- WATER METER
- WATER VALVE
- FIRE HYDRANT
- GAS LINE
- GAS VALVE
- EFFLUENT WATER LINE
- EFFLUENT WATER LINE MANHOLE
- OVERHEAD POWER
- UNDERGROUND COMMUNICATIONS
- TRAFFIC SIGNAL POLE
- PULL BOX
- STORM DRAIN
- STORM DRAIN MANHOLE (EXISTING)
- STORM DRAIN MANHOLE (PROPOSED)
- STORM DRAIN FLARED END SECTION
- EX. CATCH BASIN
- CURB & GUTTER
- CONTROL POINT
- BENCH MARK
- TELEPHONE MANHOLE
- TELEPHONE LINE
- ELECTRIC FACILITIES (MANHOLE)
- UTILITY POLE W/ LIGHT
- LIGHT POLE
- GUARDRAIL
- FENCE
- BOLLARD
- PROPERTY LINE
- CENTERLINE
- RIGHT OF WAY
- GRADE BREAK
- FLOWLINE

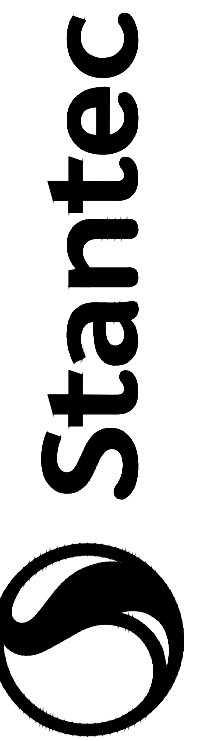
NOTE: ALL SYMBOLS OR ABBREVIATIONS MAY NOT BE USED ON PLANS

ABBREVIATIONS

- AC ASPHALTIC CEMENT
- A.D. ALGEBRAIC DIFFERENCE
- AP ANGLE POINT
- APP ASPHALT PAVEMENT PATH
- ARV AIR RELEASE VALVE
- BC BEGINNING OF CURVE
- BF BOTTOM OF FOOTING
- BFC BACK FACE OF CURB
- BVCS BEGINNING OF VERTICAL CURVE STATION
- BW/BOW BACK OF SIDEWALK
- CB CATCH BASIN
- C or CL CENTERLINE
- CMP CORRUGATED METAL PIPE
- CONC. CONCRETE
- CONST. CONSTRUCT
- DI DROP INLET
- D.I.P. DUCTILE IRON PIPE
- EC END OF CURVE
- EG EXISTING GROUND
- ELEV. ELEVATION
- EP EDGE OF PAVEMENT
- EVCE END OF VERTICAL CURVE ELEVATION
- EVCS END OF VERTICAL CURVE STATION
- EX EXISTING
- EX EXISTING
- FF FINISH FLOOR
- FFC FRONT FACE OF CURB
- FG FINISH GRADE
- FH FIRE HYDRANT
- FL FLOWLINE
- FLG FLANGED
- FT FEET
- FV FLUSH VALVE
- G GAS
- GB GRADE BREAK
- HORIZ. HORIZONTAL
- HW HEAD WALL
- IE INVERT ELEVATION
- K RATE OF VERTICAL CURVATURE
- L LENGTH
- LAT. LATERAL
- LF LINEAL FEET
- LP LOW POINT
- LT LEFT
- M.D.D. MAXIMUM DRY DENSITY
- MIN. MINIMUM
- MJ MECHANICAL JOINT
- MPOC MID POINT OF CURVE
- PC POINT OF CURVATURE
- P.C.C. PORTLAND CEMENT CONCRETE
- PCC POINT OF COMPOUND CURVATURE
- PI POINT OF INTERSECTION
- POC POINT ON CURVE
- POT POINT ON TANGENT
- PPB PEDESTRIAN PUSH BUTTON
- PRC POINT OF REVERSE CURVATURE
- PT POINT OF TANGENCY
- PVC POLYVINYL CHLORIDE
- PVI POINT OF VERTICAL INTERSECTION
- R RADIUS
- RCP REINFORCED CONCRETE PIPE
- REF. REFERENCE
- RET RETURN
- RP RADIUS POINT
- RT RIGHT
- R/W RIGHT OF WAY
- S SLOPE
- SD STORM DRAIN
- SDMH STORM DRAIN MANHOLE
- SF SQUARE FEET
- SS SANITARY SEWER
- SSMH SANITARY SEWER MANHOLE
- SSPWC STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION
- STA STATION
- SUP SHARED USE PATH
- SW SIDEWALK
- TC TOP OF CURB
- TDC TOP OF DEPRESSED CURB
- TP TOP OF PAVEMENT
- TYP. TYPICAL
- VERT. VERTICAL
- V.C. VERTICAL CURVE
- V.P.I. VERTICAL POINT OF INTERSECTION
- W WATER
- W WALK

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Revision	By	App'd	Y1.MMM.DD
1			21.04.07
			Y1.MMM.DD

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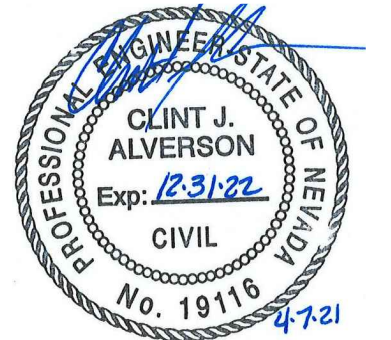
Client/Project
CITY OF SPARKS

2021 PCC ALLEYS

Sparks, NV

GENERAL NOTES, LEGEND, AND ABBREVIATIONS

Permit-Seal

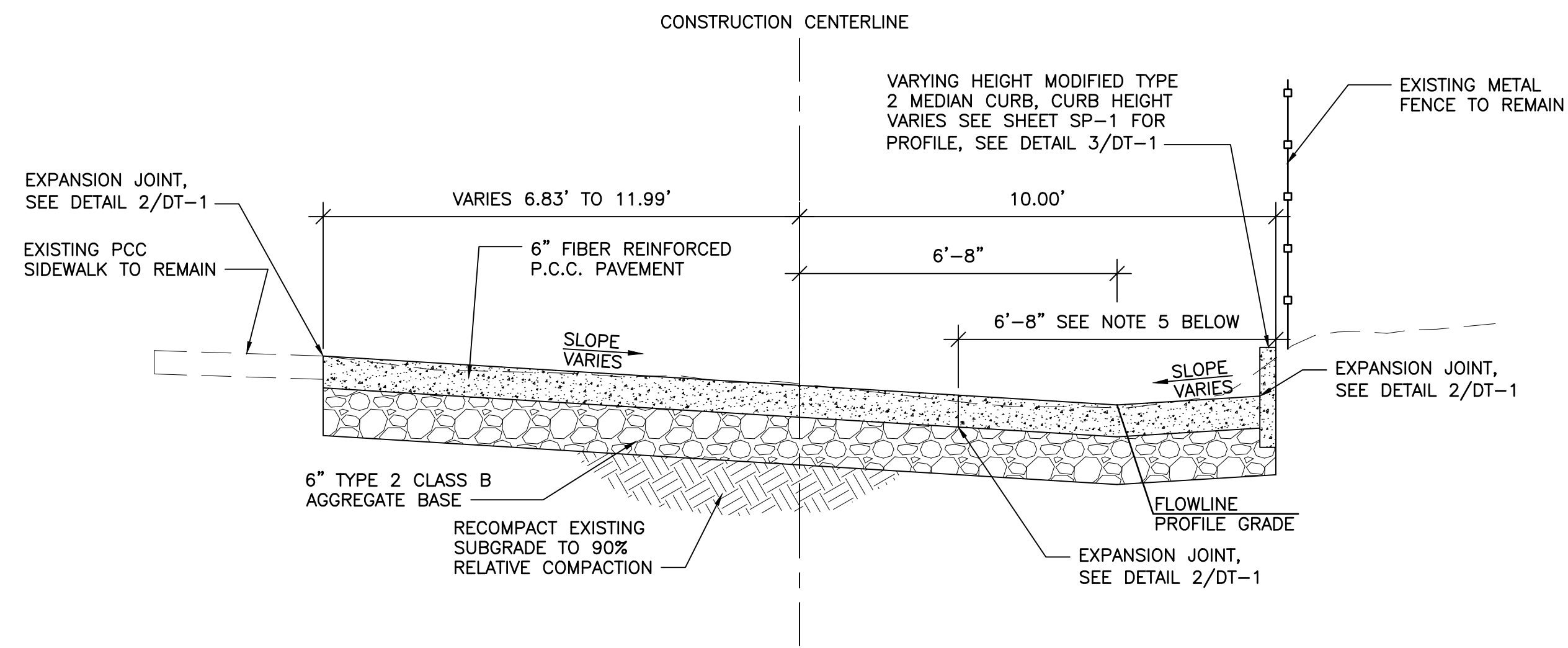


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Dwn	Chkcd	Dsgn	YYMMDD

Drawing No. C-1

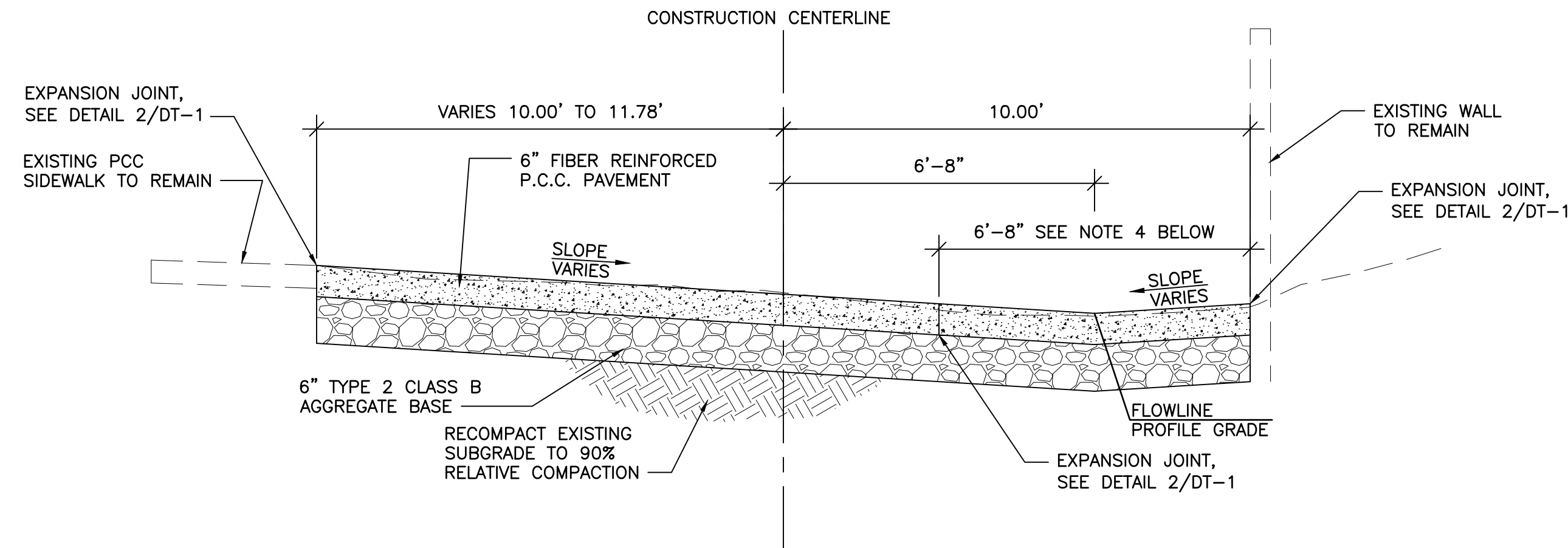
Revision Sheet



**PROPOSED SECTION OF IMPROVEMENT – FLOWLINE OFFSET TO CENTERLINE
ALLEY "C" STA. 9+98.57 TO STA. 11+62.73**

NOTES:

- PCC PAVEMENT SHALL BE JOINTED AT 6'-8" ON CENTER FOR LONGITUDINAL JOINTS AND 8'-0" ON CENTER FOR TRANSVERSE JOINTS. DEVIATIONS FROM THIS LAYOUT MUST BE APPROVED BY THE ENGINEER. JOINTS TO BE CUT A MINIMUM OF 4 HOURS TO A MAXIMUM OF 12 HOURS AFTER CONCLUSION OF BRUSH FINISHING. SEE DETAIL A, THIS SHEET.
- SEE SPECIFICATIONS FOR MIX DESIGN.
- AT STA. 9+98.57 INTERFACE WITH THE EXISTING PCC DRIVEWAY SHALL BE DOWELED WITH #4 BAR IF THE EXISTING DRIVEWAY THICKNESS IS 6" OR GREATER.
- FIBER REINFORCEMENT SHALL BE TUF STRAND, SEE SPECIAL TECHNICAL SPECIFICATIONS FOR DOSAGE.
- CONTRACTOR TO POUR VALLEY GUTTER LIMITS AS SHOW ON PLAN ABOVE FIRST AND SEPARATELY FROM REMAINING PCC WIDTH.

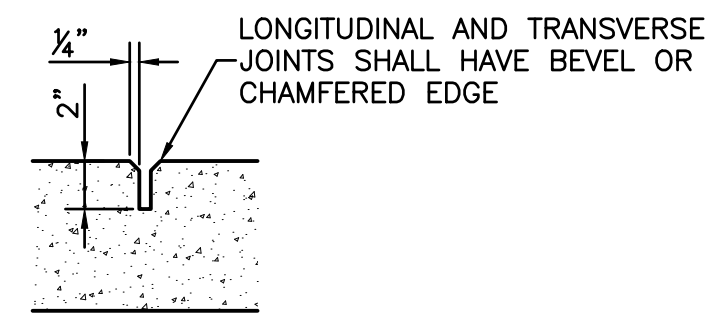


**PROPOSED SECTION OF IMPROVEMENT – FLOWLINE OFFSET TO CENTERLINE
ALLEY "C" STA. 11+62.73 TO STA. 12+77.39**

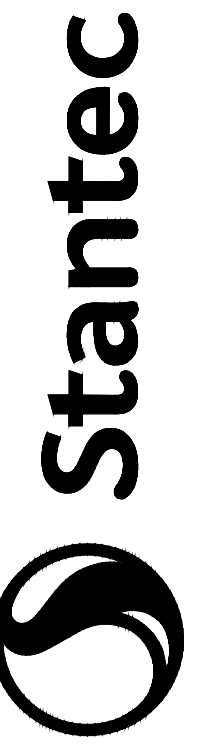
NOTES:

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- SEE SPECIFICATIONS FOR MIX DESIGN.
- FIBER REINFORCEMENT SHALL BE TUF STRAND, SEE SPECIAL TECHNICAL SPECIFICATIONS FOR DOSAGE.
- CONTRACTOR TO POUR VALLEY GUTTER LIMITS AS SHOW ON PLAN ABOVE FIRST AND SEPARATELY FROM REMAINING PCC WIDTH.

ALLEY "C" LIP/EP ELEVATIONS AND SLOPE FROM FLOWLINE						
LT. OFFSET	LT. ELEV	LT. SLOPE	STATION	RT. SLOPE	RT. ELEV	RT. OFFSET
13.50	4417.49	1.24%	10+00.00	0.03%	4417.14'	2.67
18.26	4418.18'	5.32%	10+25.00	2.00%	4417.27	2.67
18.32	4418.26'	5.31%	10+50.00	2.00%	4417.34	2.67
18.21	4418.38'	5.58%	10+75.00	2.00%	4417.42	2.67
18.46	4418.11'	3.63%	11+00.00	2.00%	4417.49	2.67
18.58	4418.33'	4.42%	11+25.00	2.00%	4417.57	2.67
18.07	4418.41'	4.54%	11+50.00	2.00%	4417.64	2.67
18.07	4418.31'	3.57%	11+75.00	2.00%	4417.73'	3.33
18.11	4418.35'	3.39%	12+00.00	2.00%	4417.80'	3.33
18.09	4418.40'	3.25%	12+25.00	2.00%	4417.88'	3.33
18.23	4418.27'	2.11%	12+50.00	2.00%	4417.95'	3.33
16.67	4418.26'	1.80%	12+75.00	2.00%	4418.03'	3.33



DETAIL A (CONTROL JOINT)



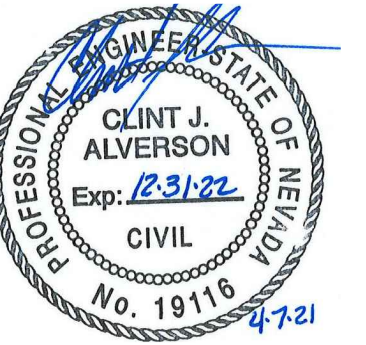
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1	ISSUED FOR BID	CJA	21.04.07
	ISSUED	YYMMDD	YYMMDD

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Client/Project
CITY OF SPARKS
2021 PCC ALLEYS
Sparks, NV

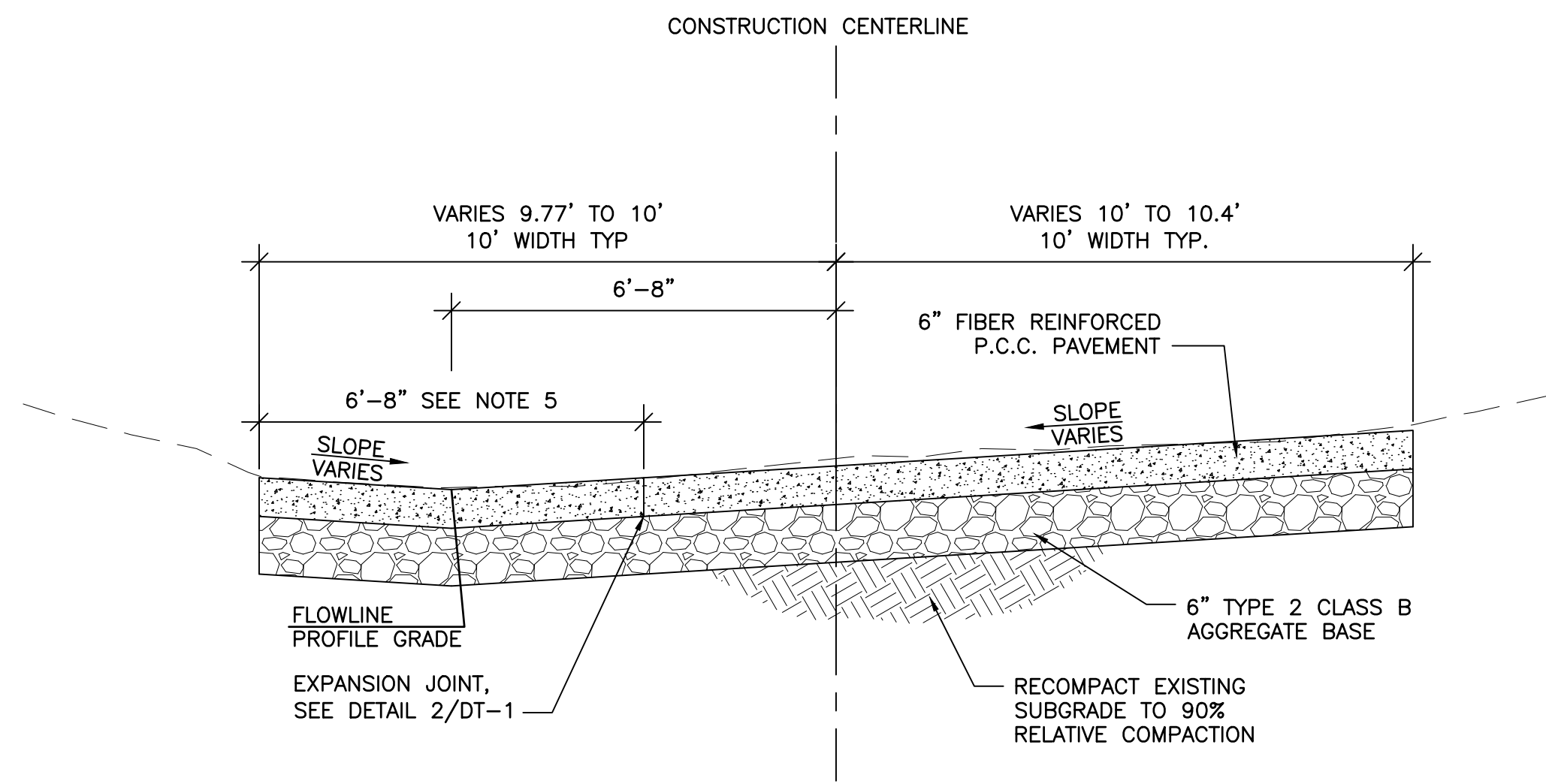
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CJA JAM CJA 21.04.01
Dwn. Chkd. Dsgn. YYMMDD
Drawing No. S1-1

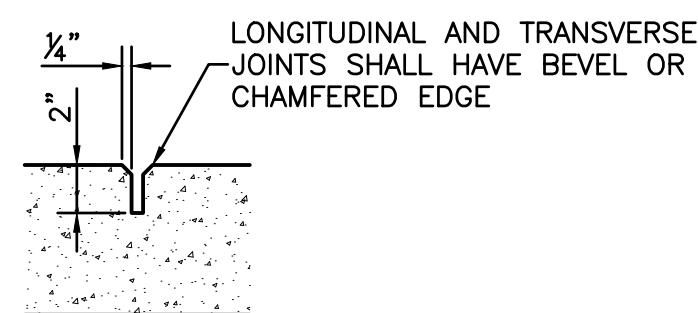
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PROPOSED SECTION OF IMPROVEMENT – FLOWLINE OFFSET TO CENTERLINE
ALLEY "D" STA. 9+98.64 TO STA. 14+01.25

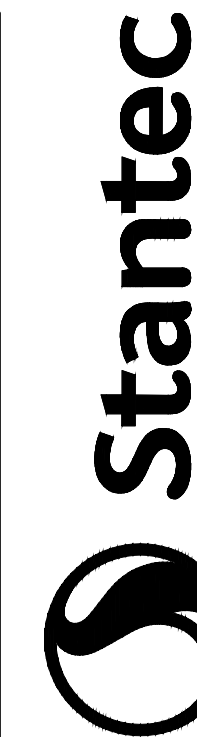
NOTES:

- PCC PAVEMENT SHALL BE JOINTED AT 6'-8" ON CENTER FOR LONGITUDINAL JOINTS AND 8'-0" ON CENTER FOR TRANSVERSE JOINTS. DEVIATIONS FROM THIS LAYOUT MUST BE APPROVED BY THE ENGINEER. JOINTS TO BE CUT A MINIMUM OF 4 HOURS TO A MAXIMUM OF 12 HOURS AFTER CONCLUSION OF BRUSH FINISHING. SEE DETAIL A, THIS SHEET.
- SEE SPECIFICATIONS FOR MIX DESIGN.
- AT STA. 9+98.64 AND STA. 14+01.25 INTERFACES WITH THE EXISTING PCC DRIVEWAYS SHALL BE DOWELED WITH #4 BAR IF THE EXISTING DRIVEWAY THICKNESS IS 6" OR GREATER.
- FIBER REINFORCEMENT SHALL BE TUF STRAND, SEE SPECIAL TECHNICAL SPECIFICATIONS FOR DOSAGE.
- CONTRACTOR TO POUR VALLEY GUTTER LIMITS AS SHOW ON PLAN ABOVE FIRST AND SEPARATELY FROM REMAINING PCC WIDTH.



DETAIL A (CONTROL JOINT)

ALLEY "D" LIP/EP ELEVATIONS AND SLOPE FROM FLOWLINE						
LT. OFFSET	LT. ELEV	LT. SLOPE	STATION	RT. SLOPE	RT. ELEV	RT. OFFSET
3.33	4418.10'	0.15%	10+00.00	0.34%	4418.15'	16.67
3.33	4418.38'	3.00%	10+25.00	3.30%	4418.83'	16.67
3.33	4418.70'	3.00%	10+50.00	0.60%	4418.70'	16.67
3.33	4418.72'	3.00%	10+75.00	1.38%	4418.85'	16.67
3.33	4418.55'	3.00%	11+00.00	2.34%	4418.84'	16.67
3.33	4418.38'	3.00%	11+25.00	1.26%	4418.82'	16.67
3.33	4418.20'	3.00%	11+50.00	1.32%	4418.31'	16.67
3.33	4417.76'	3.00%	11+75.00	0.48%	4417.88'	16.67
3.33	4417.42'	3.00%	12+00.00	1.44%	4417.40'	16.67
3.33	4417.29'	3.00%	12+25.00	3.18%	4417.43'	16.67
3.33	4417.17'	3.00%	12+50.00	3.42%	4417.60'	16.67
3.33	4417.05'	3.00%	12+75.00	3.72%	4417.52'	16.67
3.33	4416.92'	3.00%	13+00.00	3.90%	4417.44'	16.67
3.27	4416.80'	3.00%	13+25.00	3.36%	4417.35'	16.67
3.14	4416.65'	3.00%	13+50.00	3.36%	4417.12'	17.00
3.16	4416.47'	3.00%	13+75.00	2.76%	4416.84'	17.06
2.98	4416.25'	0.01%	14+00.00	-0.63%	4416.09'	16.67



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1	ISSUED FOR BID	CJA	21	04	07
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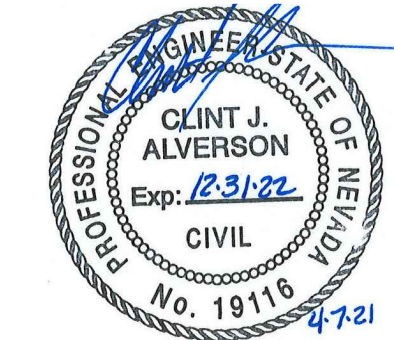
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CITY OF SPARKS

Title
2021 PCC ALLEYS

Sparks, NV

Permit-Seal



Project Number: 180101424
File Name: 01424_SA-D_S1.dwg

CJA	JAM	CJA	21.04.01
Dwn.	Chkd.	Dsgn.	YYMMDD

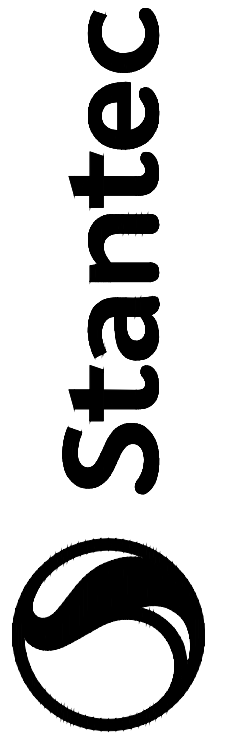
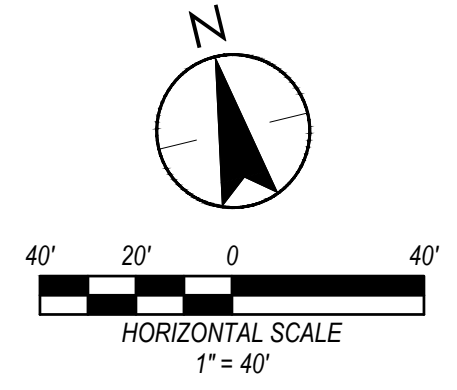
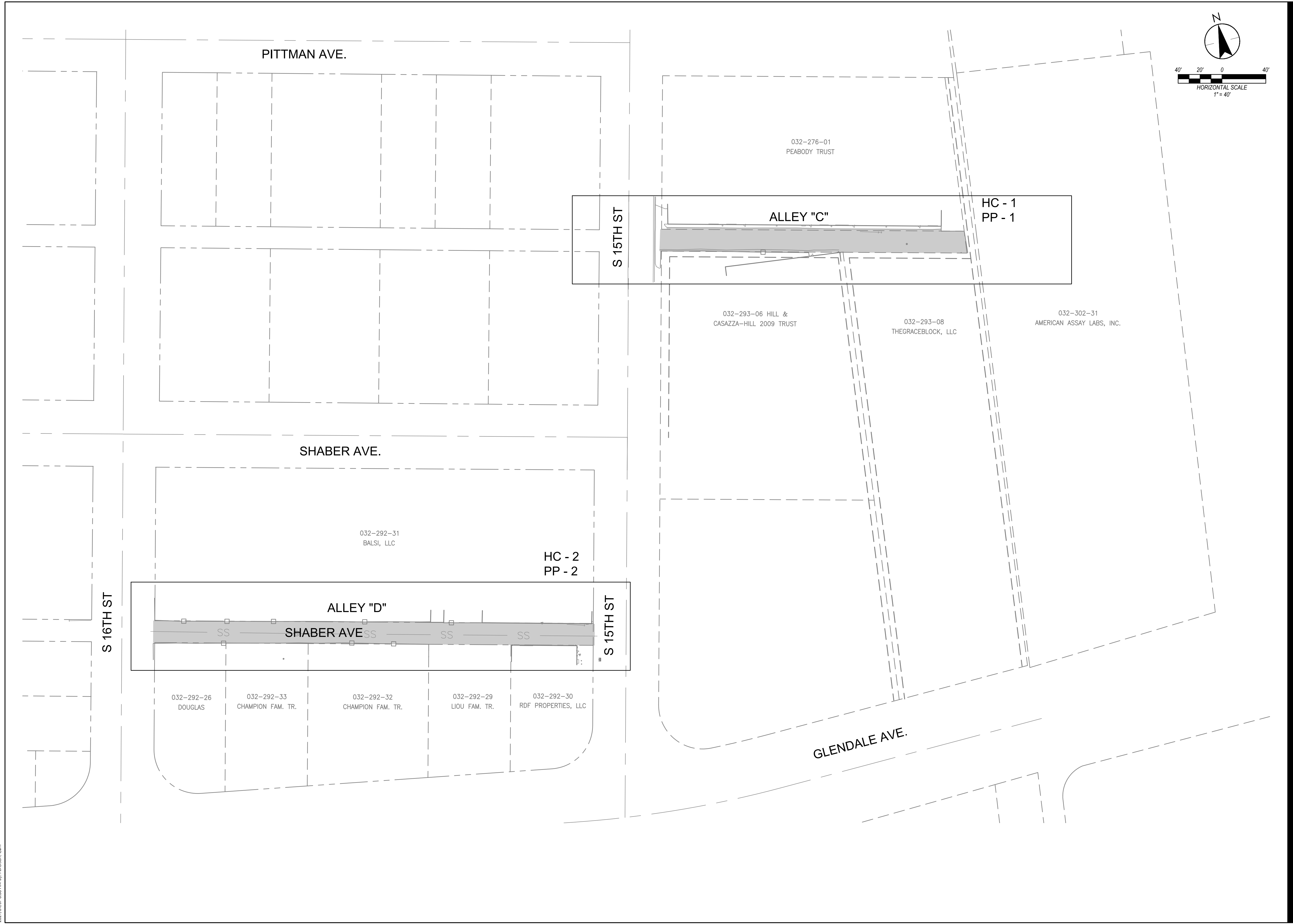
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Revision Sheet

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4 of 12

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 2021.04.07 09:59 AM By: ALVERSON, J



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1		CJA	21.04.07
		By	Y1.MM.DD

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CITY OF SPARKS

2021 PCC ALLEYS

Sparks, NV

Title
KEY SHEET



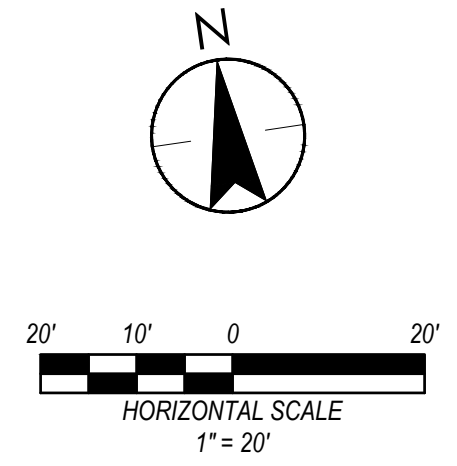
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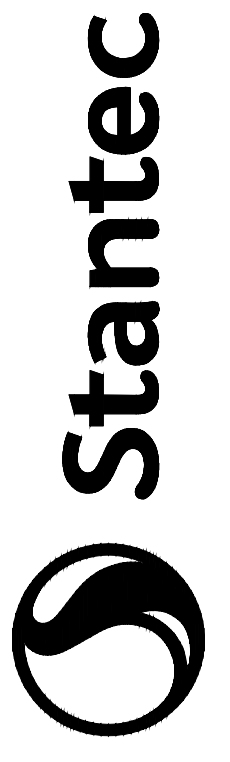
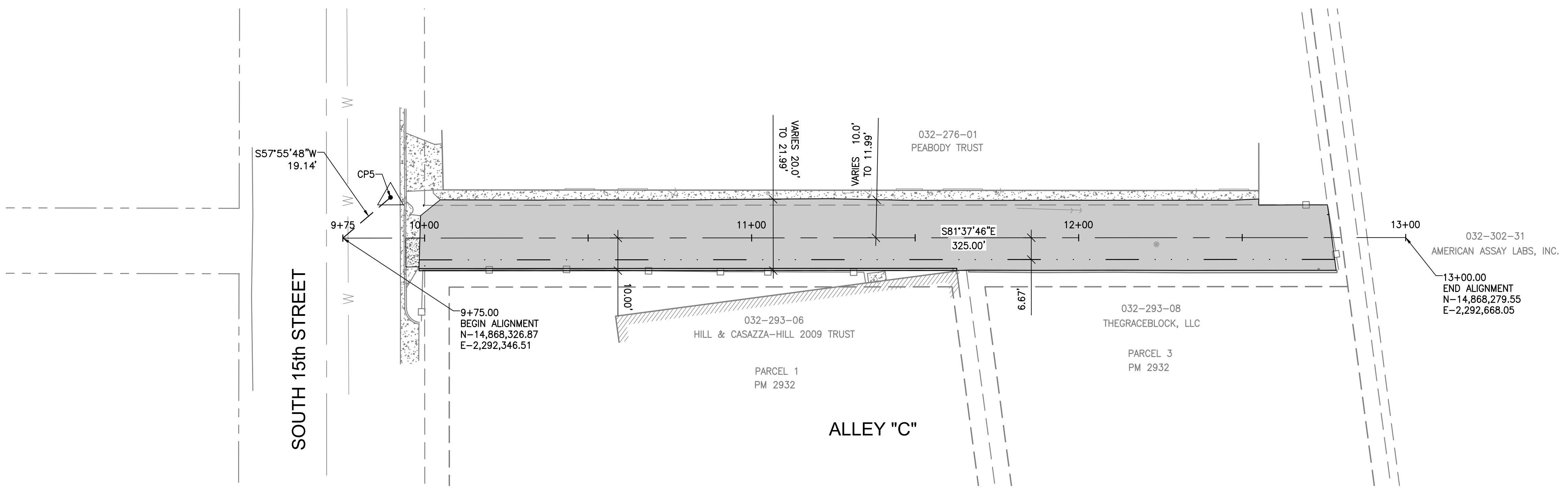
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Dwn.	Chkd.	Dsgn.	Y1.MM.DD

Drawing No. SP-1

Revision Sheet



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CP5	14,868,337.03	2,292,362.73	4417.03	SET MAG WP - PITT	09+89.57	12.42 LT.



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			Y1.MM.DD

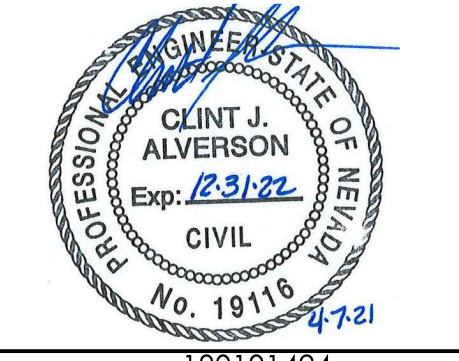
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Title
 2021 PCC ALLEYS

Sparks, NV

Permit-Seal
 ALLEY C - HORIZONTAL CONTROL PLAN



Project Number: 180101424
 File Name: 01424_SA-C_HC.dwg

CJA	JAM	CJA	21.04.01
Dwn.	Chkd.	Dsgn.	Y1.MM.DD

Drawing No. HC-1

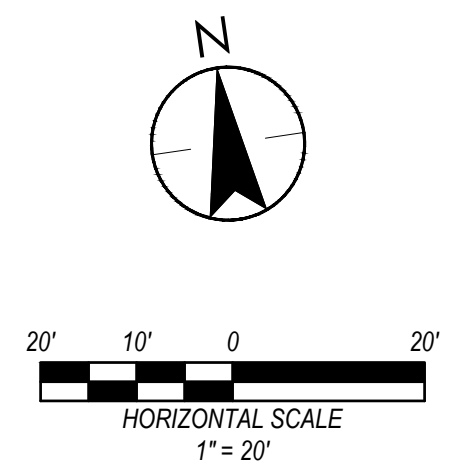
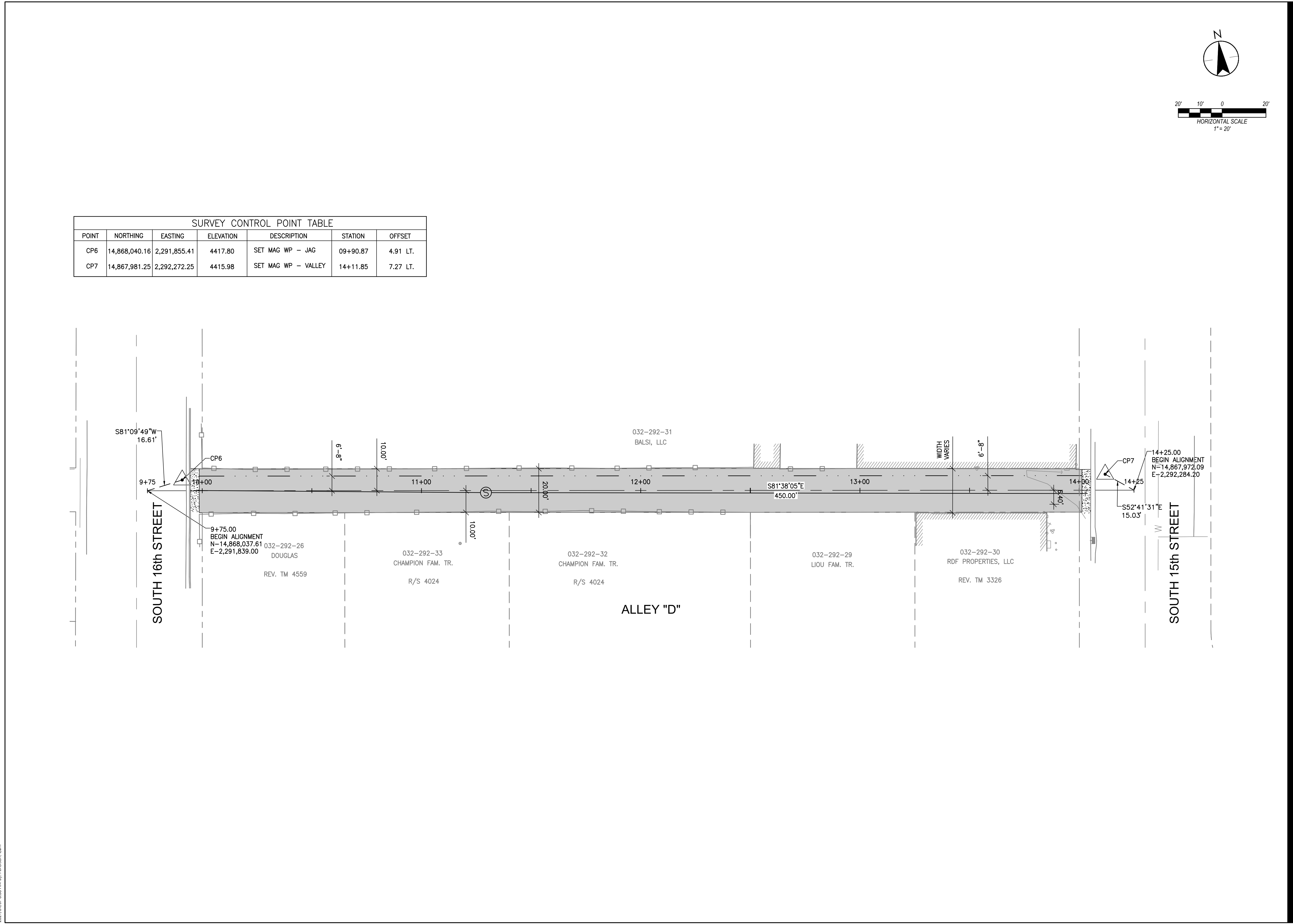
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0 6 of 12

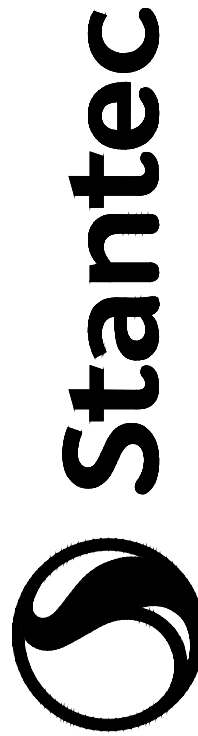
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CP7	14,867,981.25	2,292,272.25	4415.98	SET MAG WP - VALLEY	14+11.85	7.27 LT.



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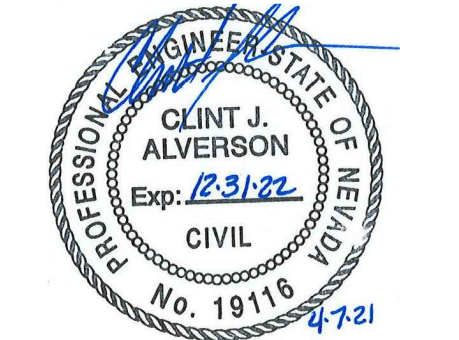
Client/Project
CITY OF SPARKS

2021 PCC ALLEYS

Sparks, NV

Title
ALLEY D - HORIZONTAL CONTROL PLAN

Permit-Seal



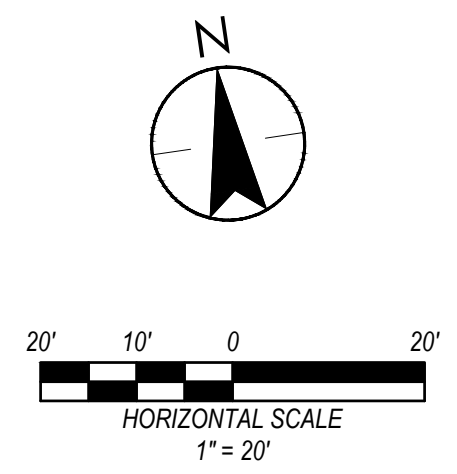
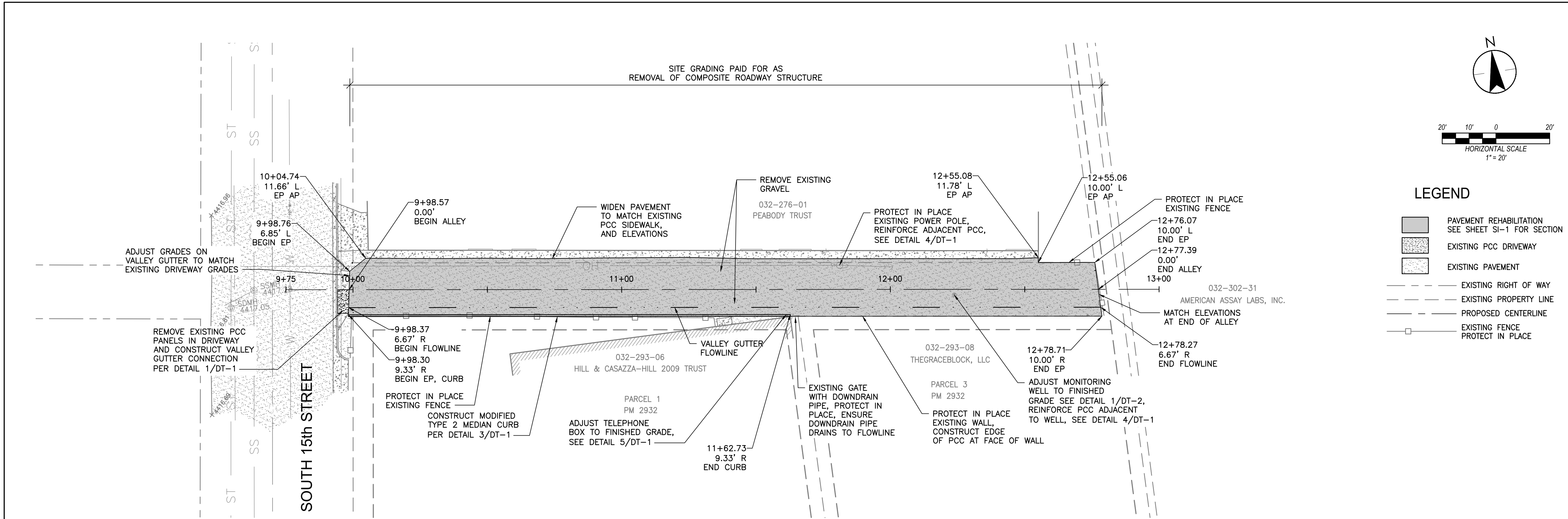
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Drawing No. HC-2

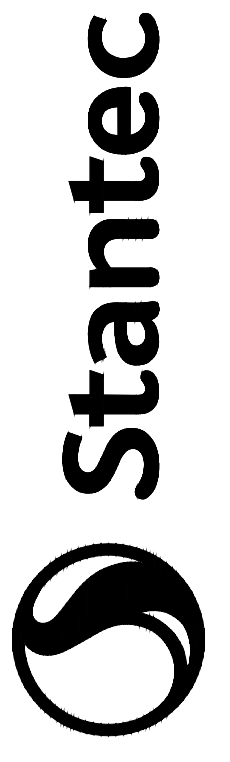
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 2021.04.07 09:59 AM By: ALVERSON, CLINT



LEGEND

- PAVEMENT REHABILITATION
SEE SHEET SI-1 FOR SECTION
- EXISTING PCC DRIVEWAY
- EXISTING PAVEMENT
- EXISTING RIGHT OF WAY
- EXISTING PROPERTY LINE
- PROPOSED CENTERLINE
- EXISTING FENCE
- PROTECT IN PLACE

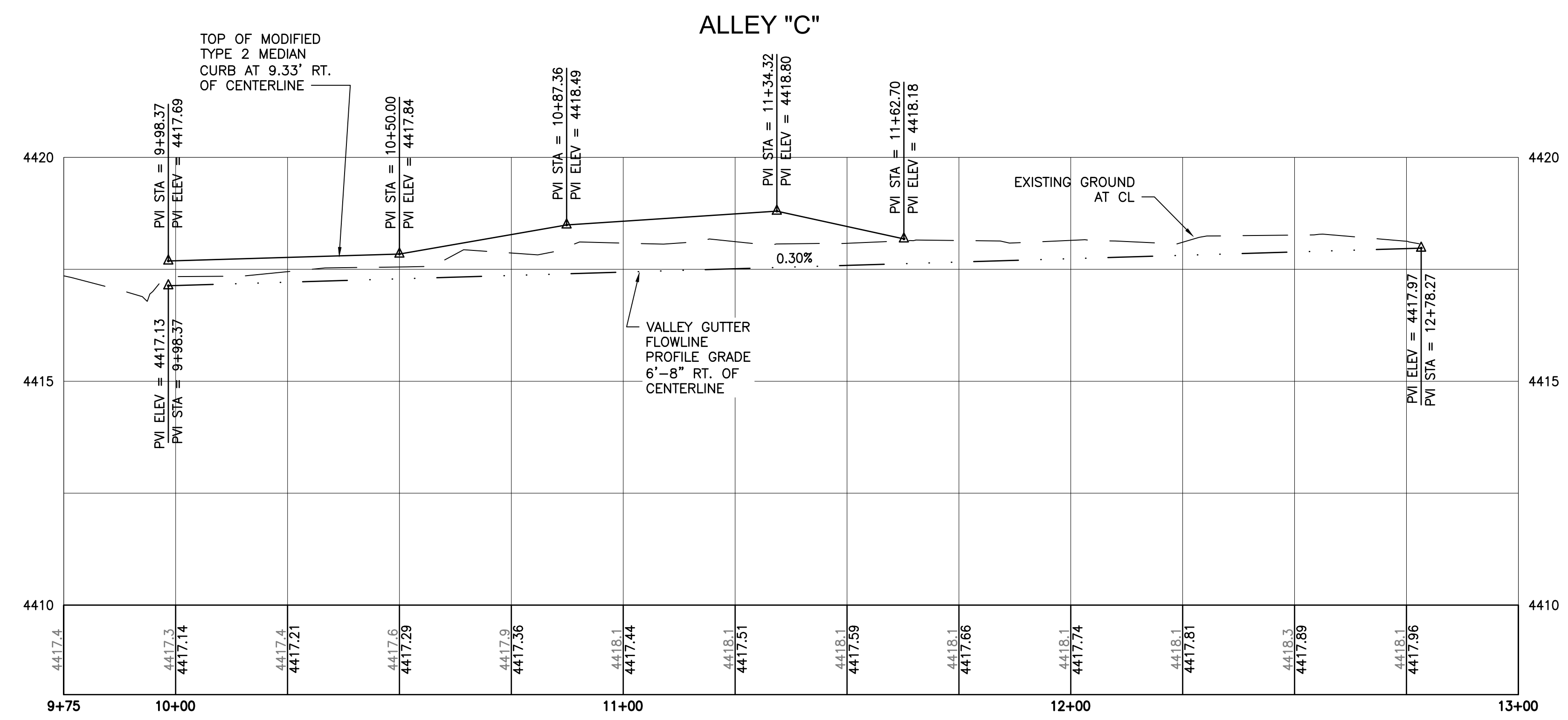


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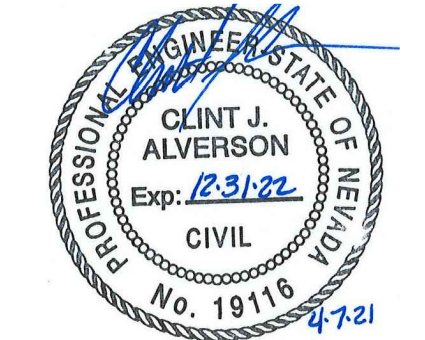
NOTES

1. EXPANSION JOINTS SHALL BE LOCATED AT ALL PCC PAVEMENT/C&G, PCC PAVEMENT/MEDIAN CURB, POWER POLES, PCC PAVEMENT/MANHOLE/CATCH BASIN, AND WALL INTERFACES. SEE DETAIL 2/DT-1.



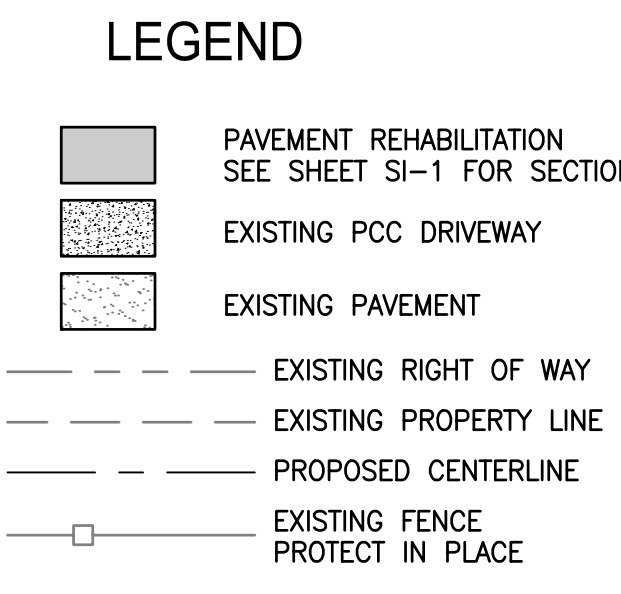
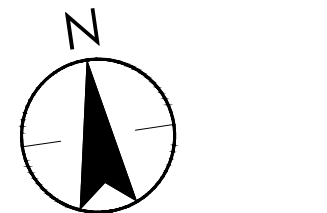
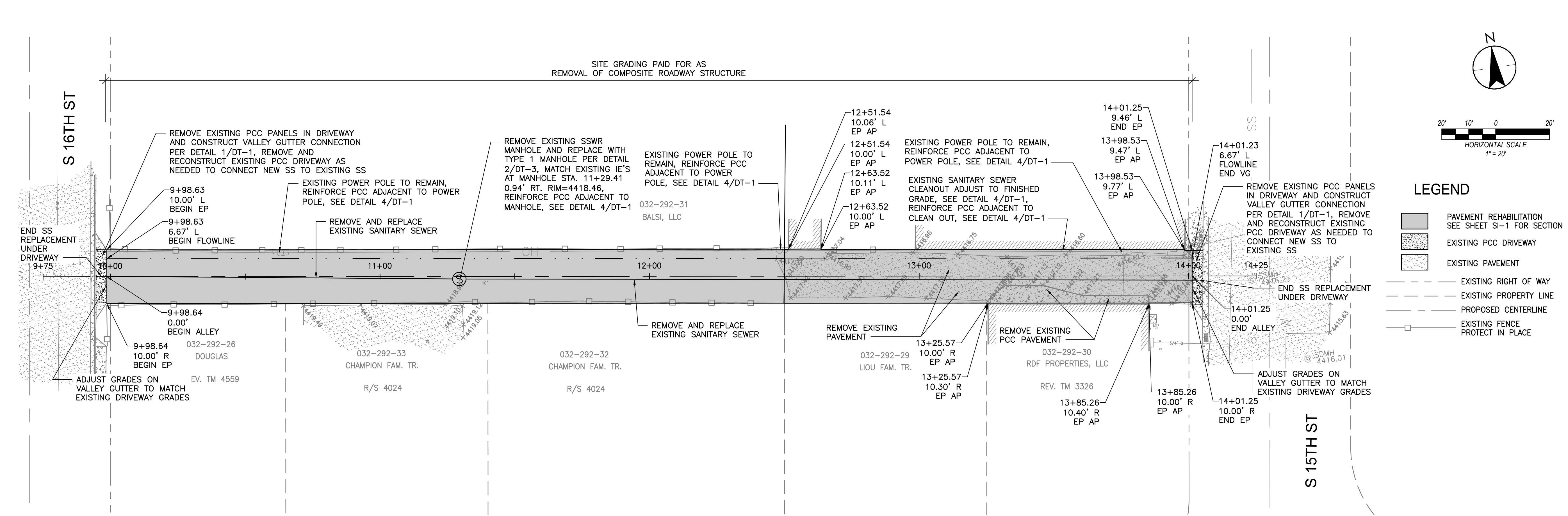
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 2021 PCC ALLEYS
 Sparks, NV
 Title
 ALLEY C - PLAN AND PROFILE



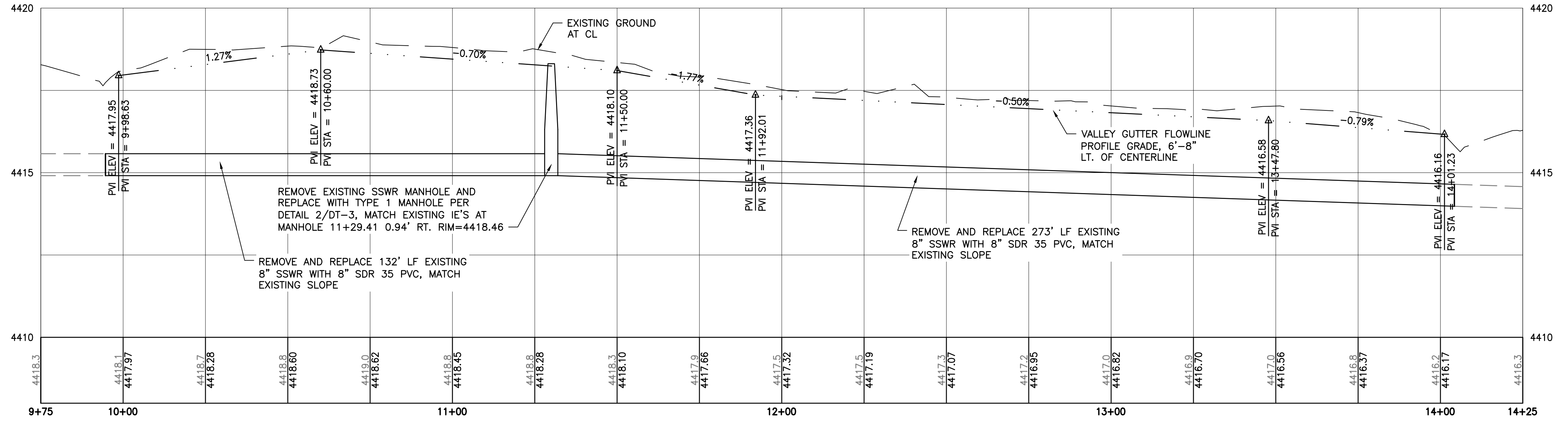
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2021.04.07 09:59 AM By: A. Alverson
ORIGINAL SHEET - ANSI D



ALLEY "D"

- NOTES**
- EXPANSION JOINTS SHALL BE LOCATED AT ALL PCC PAVEMENT/C&G, PCC PAVEMENT/MEDIAN CURB, POWER POLES, AND PCC PAVEMENT/MANHOLE/CATCH BASIN INTERFACES. SEE DETAIL 2/DT-1
 - SANITARY SEWER LATERALS TO BE RECONNECTED TO NEW PIPE PER DETAIL 5/DT-3



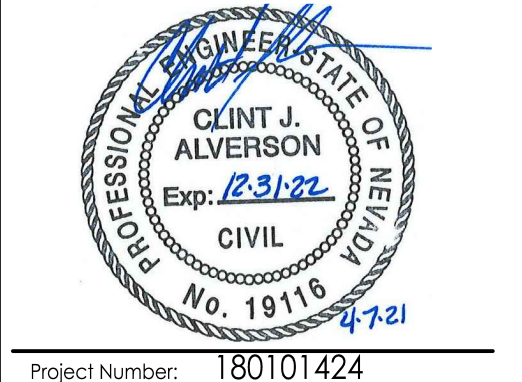
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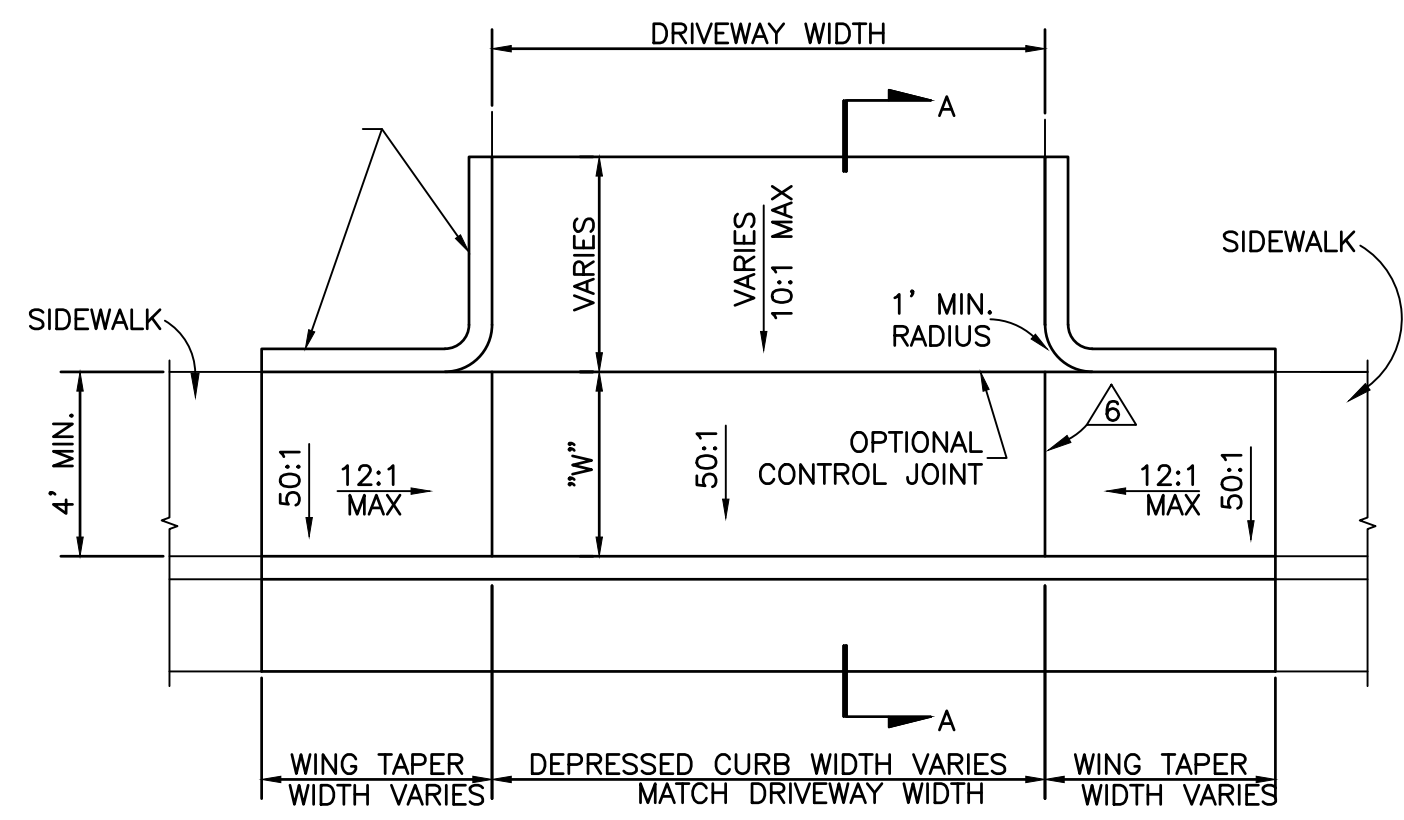
Client/Project
CITY OF SPARKS
2021 PCC ALLEYS
Sparks, NV

Title
ALLEY D - PLAN AND PROFILE

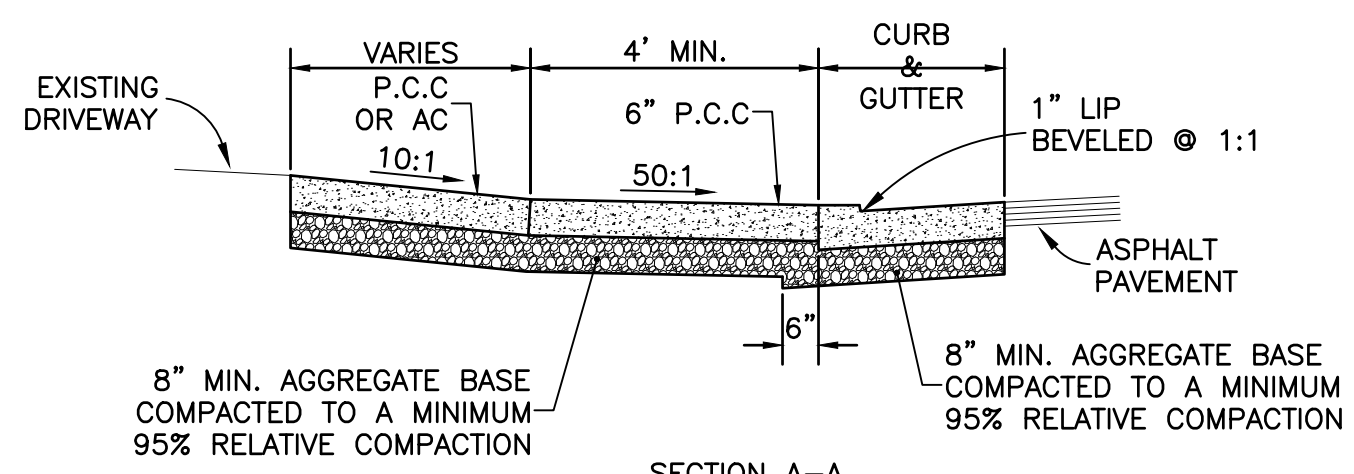


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File Name: 01424_SA-D_PP.dwg

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Drawing No.			PP-2
Revision		Sheet	

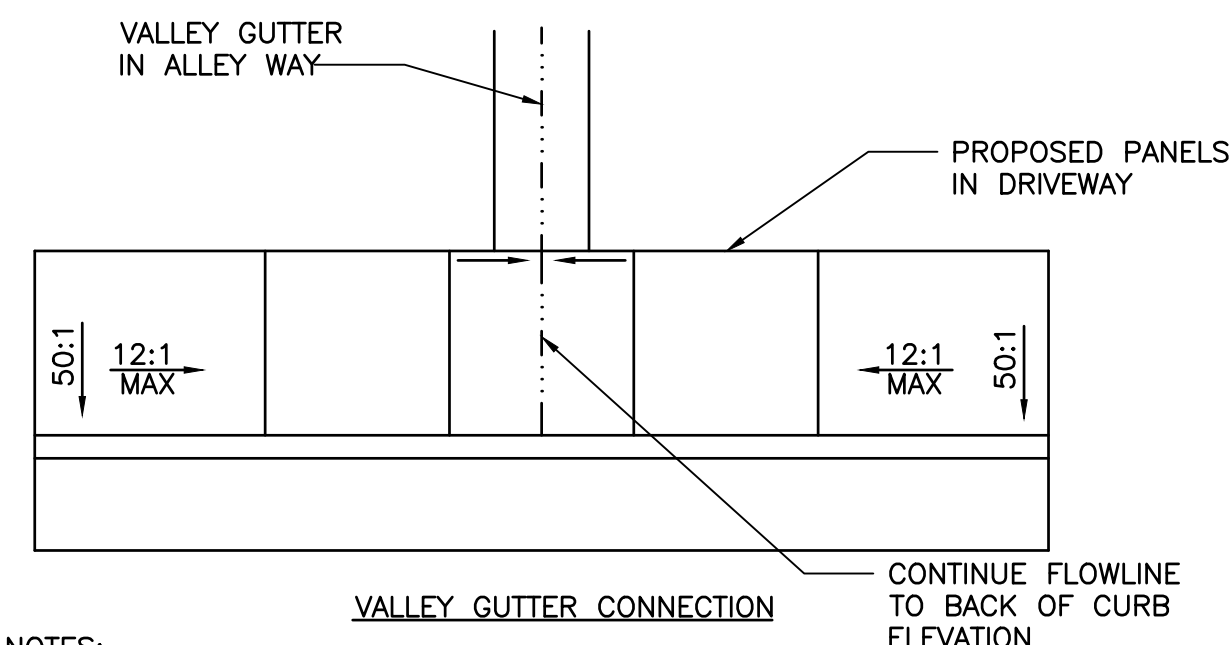


PLAN



SECTION A-A

1 COMMERCIAL DRIVEWAY
DT-1 NTS

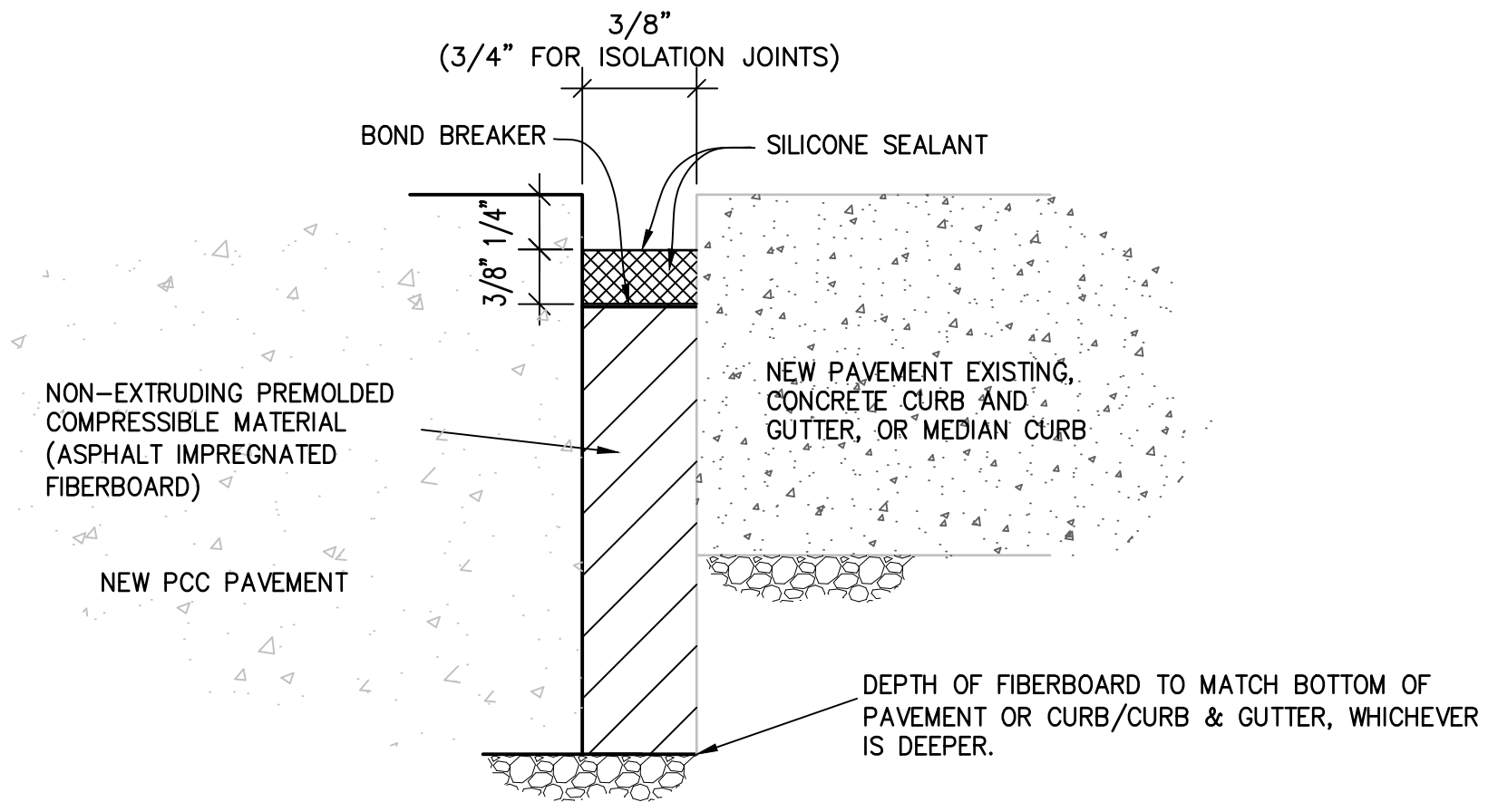


VALLEY GUTTER CONNECTION

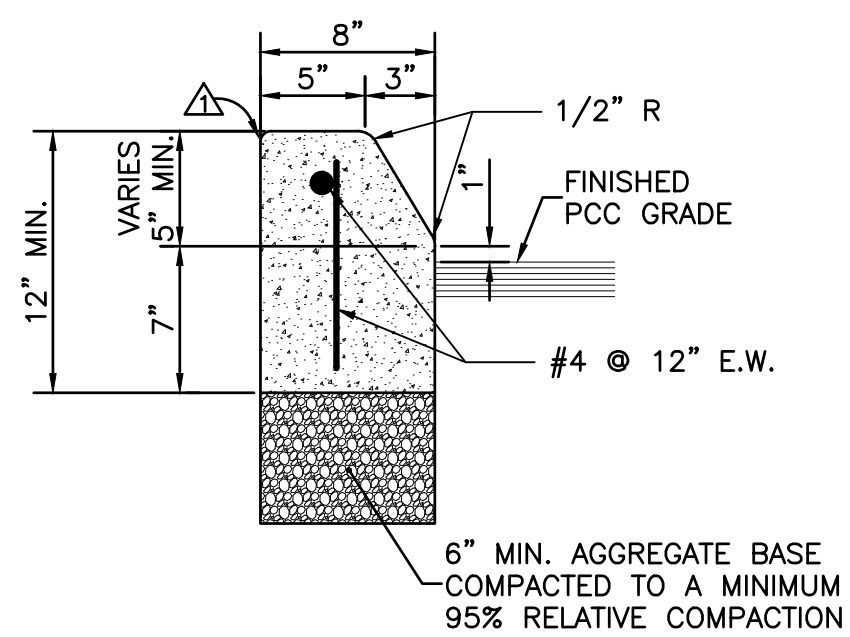
NOTES:

- SEE SPECIFICATIONS FOR CONCRETE MIX DESIGN.
- AGGREGATE BASE MATERIAL UNDER DRIVEWAYS AND SIDEWALKS SHALL BE TYPE 2, CLASS B CRUSHED AGGREGATE BASE. ALL MATERIALS SHALL CONFORM TO SSPWC SECTION 200.
- COMMERCIAL DRIVEWAYS MAY BE POURED MONOLITHIC WITH CURB AND GUTTER. COMMERCIAL DRIVEWAYS TO HAVE #4 BARS AT 18" ON CENTER LONGITUDINAL & TRANSVERSE EXTENDING INTO GUTTER PAN AND DRIVEWAY WINGS. MINIMUM 2" CONCRETE COVER FOR ALL REINFORCING BARS. WHEN COMMERCIAL DRIVEWAY APPROACH AND CURB & GUTTER IS POURED SEPARATELY, IT SHALL BE REQUIRED FOR EACH REINFORCING BAR TO BE DOWELED INTO ADJACENT CURB & GUTTER. DOWELS SHALL #4 REBAR, PENETRATE INTO CURB & GUTTER MINIMUM OF 6", SPACED AT 18" ON CENTER AND BE SECURELY TIED TO THE DRIVEWAY APPROACH REINFORCING.
- IF JOINT EXISTS WITHIN 4 FEET OF DRIVEWAY, REMOVE SIDEWALK AND CURB AND GUTTER TO THAT JOINT.
- ALL ADJACENT CONCRETE REMOVAL SHALL BE TO NEAT SAW CUT LINES AT RIGHT ANGLES. DOWEL INTO EXISTING ADJACENT DRIVEWAY APPROACH OR SIDEWALK WITH (2) No. 4 REINFORCEMENT BARS EQUALLY SPACED ACROSS WIDTH "W". DOWELS SHALL PENETRATE A MINIMUM OF 4" INTO EXISTING CONCRETE.

WEAKENED PLANE JOINTS SHALL BE CONSTRUCTED AT 5 FT INTERVALS AND IN ACCORDANCE WITH SECTION 312 OF THE SSPWC.



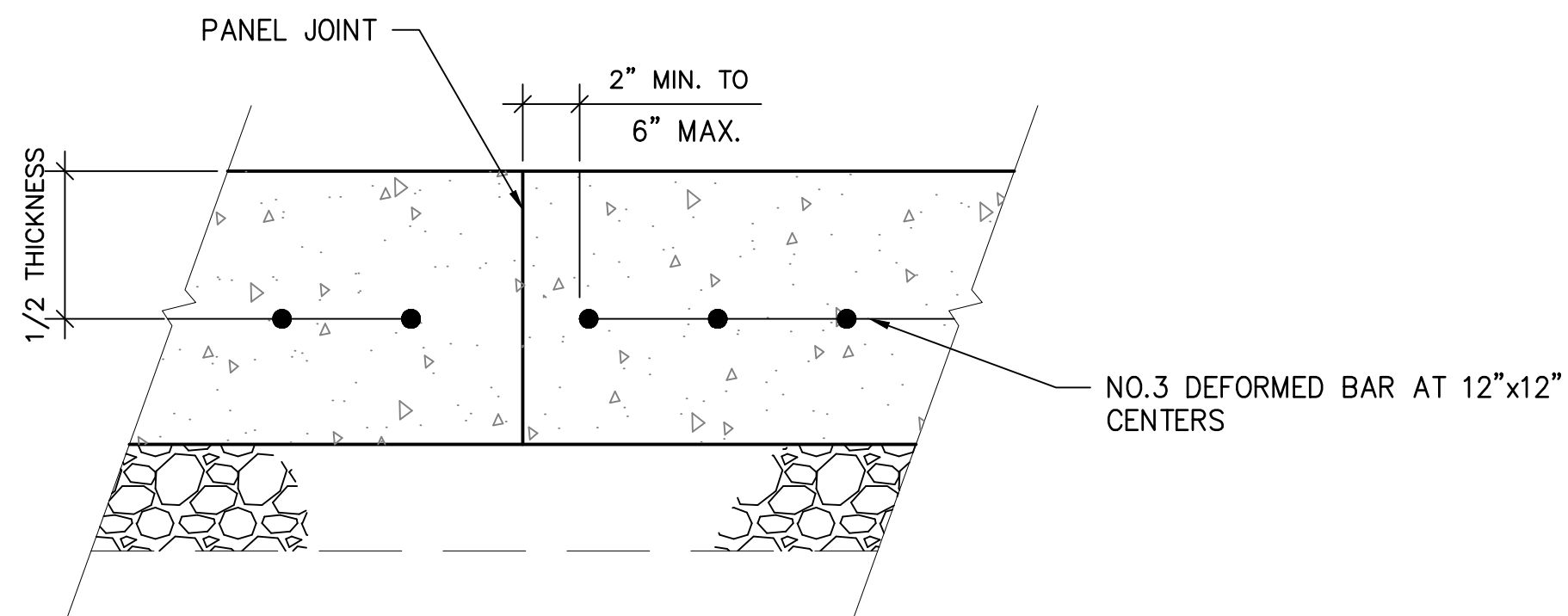
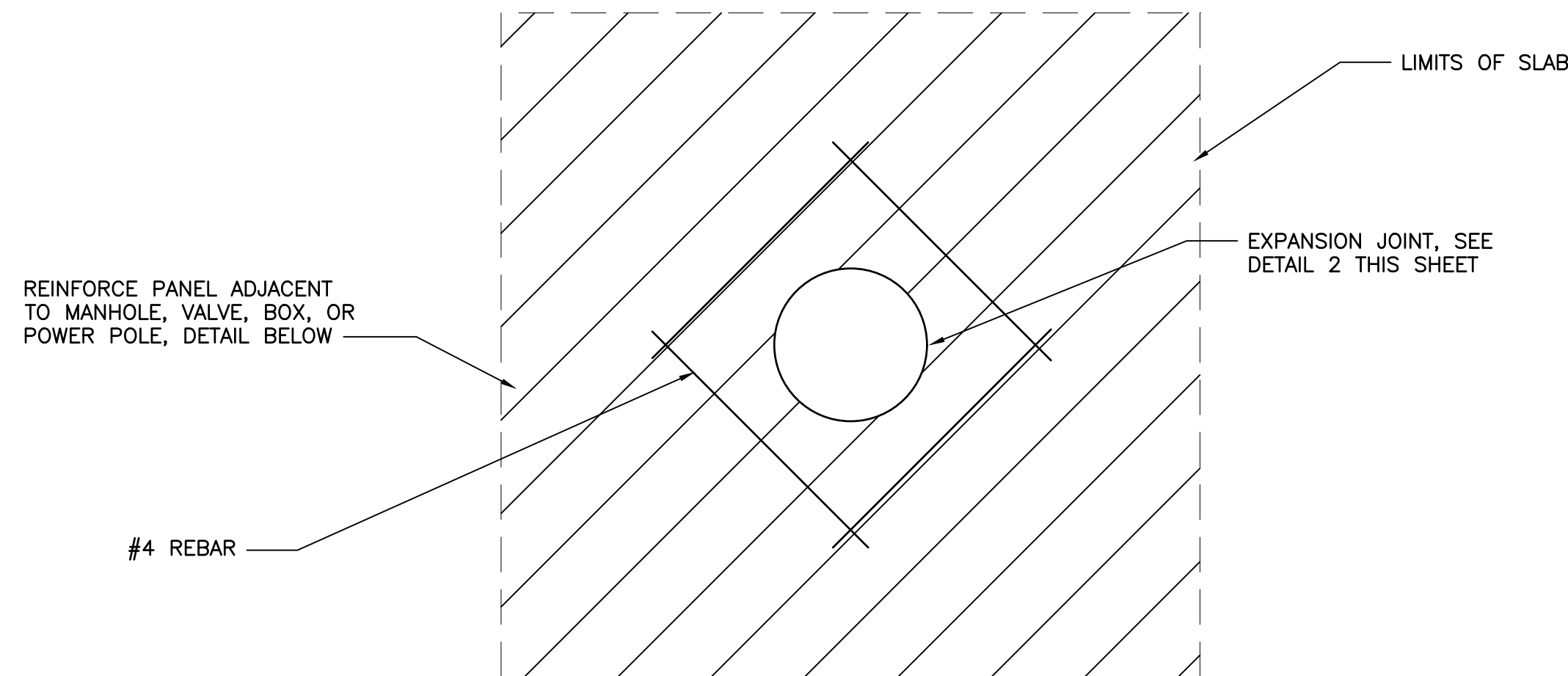
2 TYPICAL EXPANSION JOINT
DT-1



NOTES:

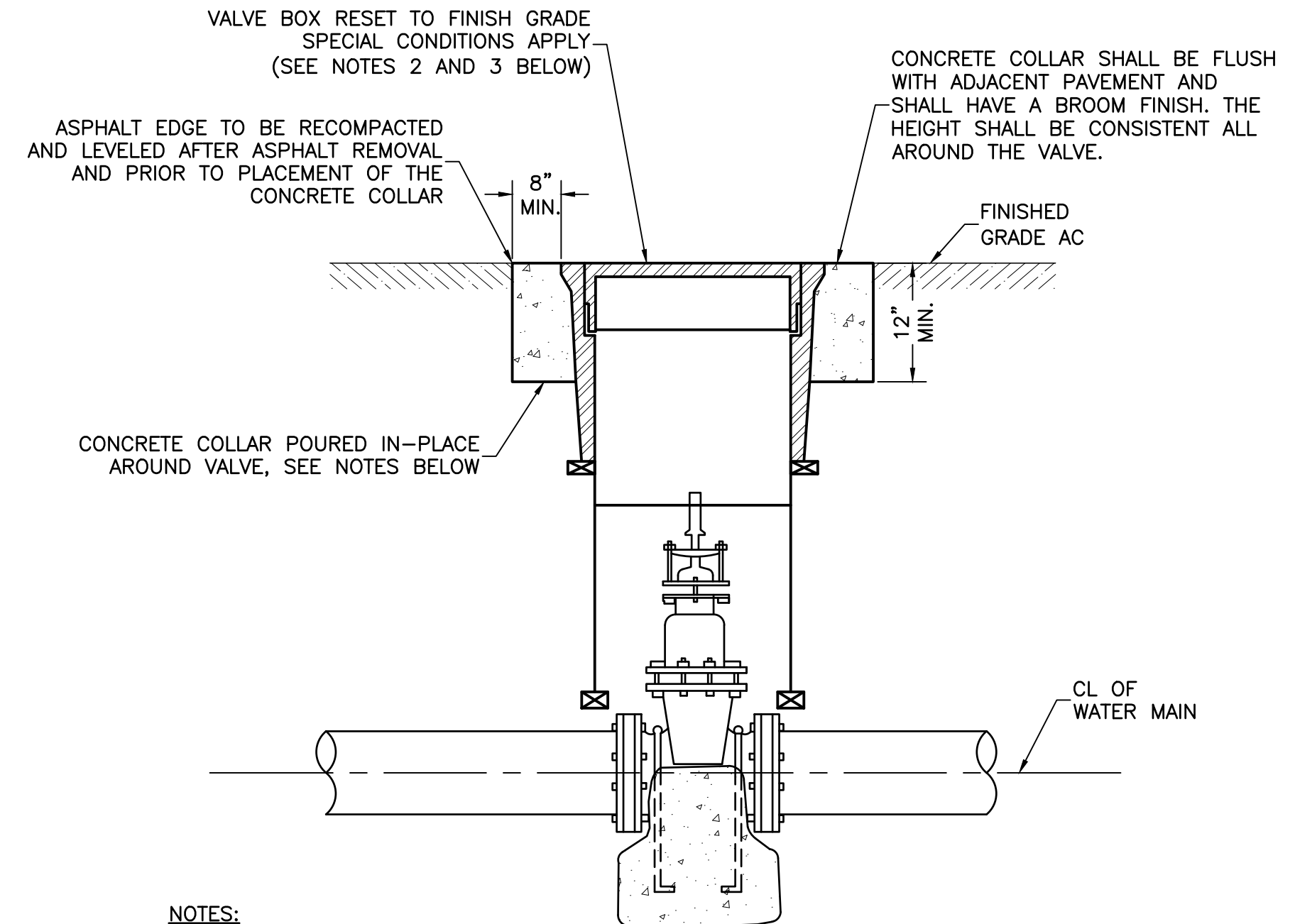
- RADIUS TO BE 1/2 INCH, OMIT ROUNDING IF CURBS ARE BACK TO BACK.
- SEE SPECIFICATIONS FOR CONCRETE MIX DESIGN.
- AGGREGATE BASE MATERIAL UNDER MEDIAN CURBS SHALL BE TYPE 2, CLASS B CRUSHED AGGREGATE BASE. MATERIALS SHALL CONFORM TO SSPWC SECTION 200.

3 MODIFIED TYPE 2 MEDIAN CURB
DT-1



REINFORCEMENT, BARS, AND EXPANSION ARE INCIDENTAL TO THE PCC PAVEMENT COST

4 REINFORCEMENT DETAIL
DT-1



NOTES:

- SEE SPECIFICATIONS FOR CONCRETE MIX DESIGN.
- SPECIAL CONDITIONS APPLY TO P.E. GAS VALVES WITH TELESCOPING RISERS. CONTRACTORS SHALL NOT RAISE TO GRADE RISERS THAT HAVE BEEN CUT-OFF DURING LOWERING. RISERS CUT-OFF DURING LOWERING SHALL BE REMOVED COMPLETELY TO MAIN AND TAKEN TO NVENERGY FOR EXACT MATERIAL REPLACEMENT PRIOR TO RAISING TO GRADE.
- SPECIAL CONDITIONS APPLY TO WATER VALVES. CONTRACTORS SHALL RAISE RISERS TO GRADE USING RISER PIPE BELLS OR COUPLERS.

5 ADJUST VALVE, UTILITY BOX TO FINISHED GRADE
DT-1

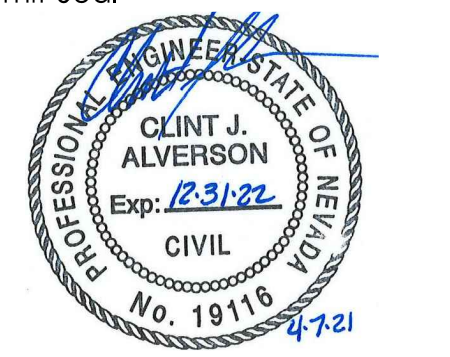
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1	ISSUED FOR BID	CJA	21.04.07
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2021 PCC ALLEYS

Sparks, NV
Title
DETAILS



Project Number: 180101424
File Name: 01424_SA-C_DT.dwg

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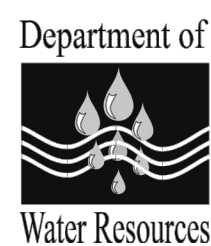
Revision Sheet

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10 of 12



Washoe County
Department of
Water Resources
4930 Energy Way
Reno, NV 89502-4106
Tel: (775) 954-4600
Fax: (775) 954-4610
http://www.washoecounty.nv.gov

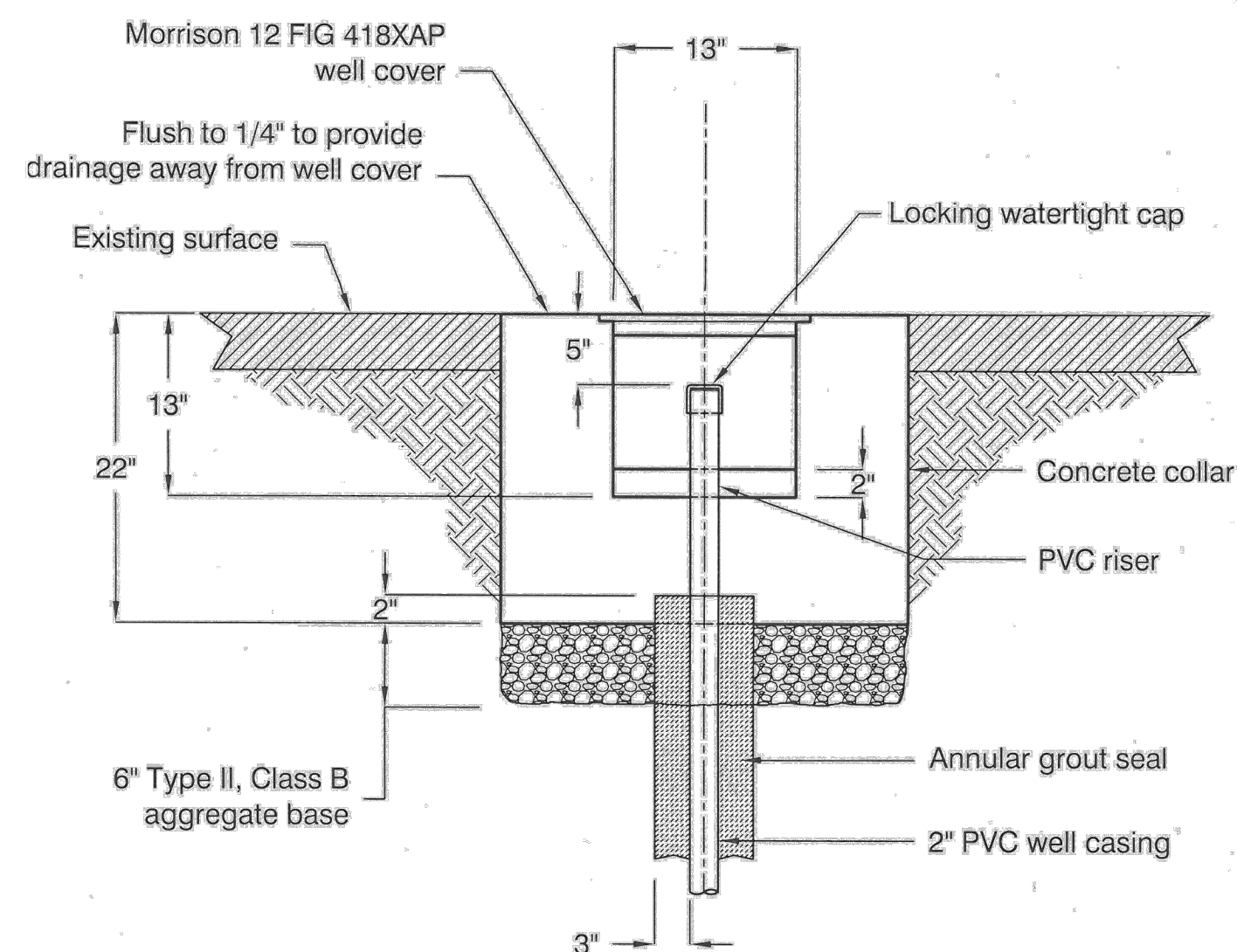


Specifications for Preserving and Reconstructing WCDWR CTMRD Program Groundwater Monitoring Wells

- Contact Tim Donahoe (954-4635/544-3826) immediately when a monitoring well is identified near the footprint of planned construction
- The Washoe County Department of Water Resources asks that Tim Donahoe (954-4635/544-3826) be contacted before a well is to be lowered or raised to provide oversight for the process.
- See the next page (Fig. 1) for the Preconstruction Condition of the well.

REQUIRED MATERIALS:

- Well Cover: Morrison 12" diameter manhole with 12" sleeve (MODEL #12 FIG 418XAP).
- Portland Cement Concrete that meets current standard detail for City or County agency overseeing construction
- ASTM F480 well casing PVC to be used in raising well after construction (It is best to save the old piece, which is cut off, and reuse it)
- PVC coupling to join buried pipe and riser to be attached
*NO PVC glue or primer are to be used.

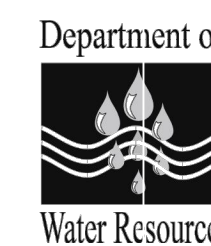


Preconstruction condition

Figure 1



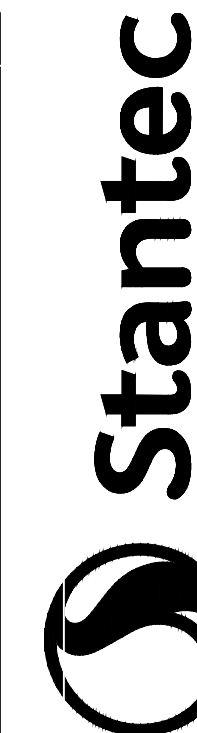
Washoe County
Department of
Water Resources
4930 Energy Way
Reno, NV 89502-4106
Tel: (775) 954-4600
Fax: (775) 954-4610
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Lowering of PVC well casing during construction

- Contact Tim Donahoe (954-4635/544-3826) before a monitoring well is to be lowered to provide oversight for the process.
- Excavate around the well casing to a depth of 23" below the existing surface.
- Cut the PVC well casing flush 19" below the ground surface, and place the water-tight cap from the top of the casing into the top and tighten.
- Expose 2" of PVC casing below the cut by removing the grout seal, but leave 2" or more of the seal exposed above the bottom of the excavation.
- See Figure 2 on the next page for details.

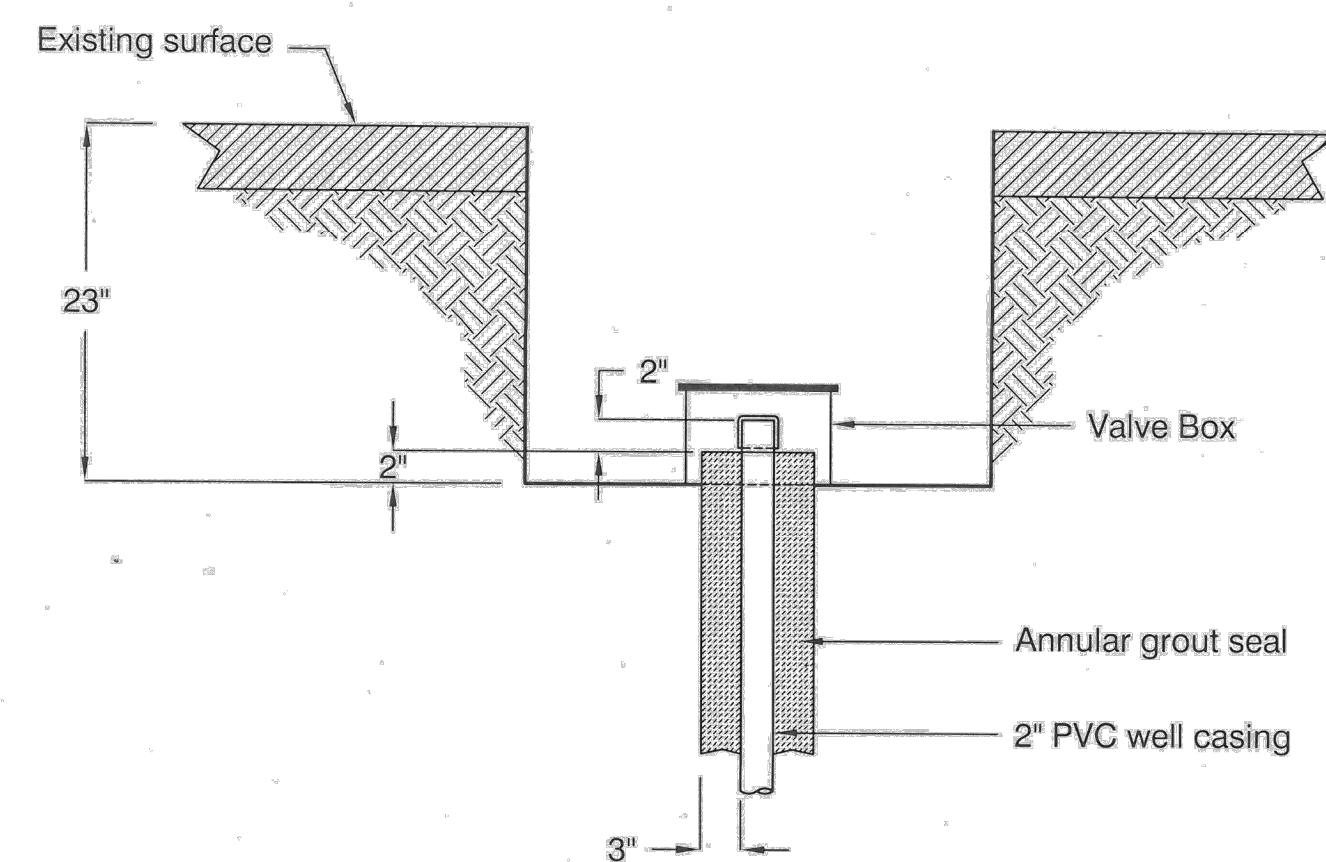
*SAVE the piece of PVC well casing to reuse in raising the well as it is a special well casing ASTM F480 standard.



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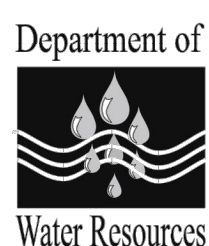


During construction

Figure 2



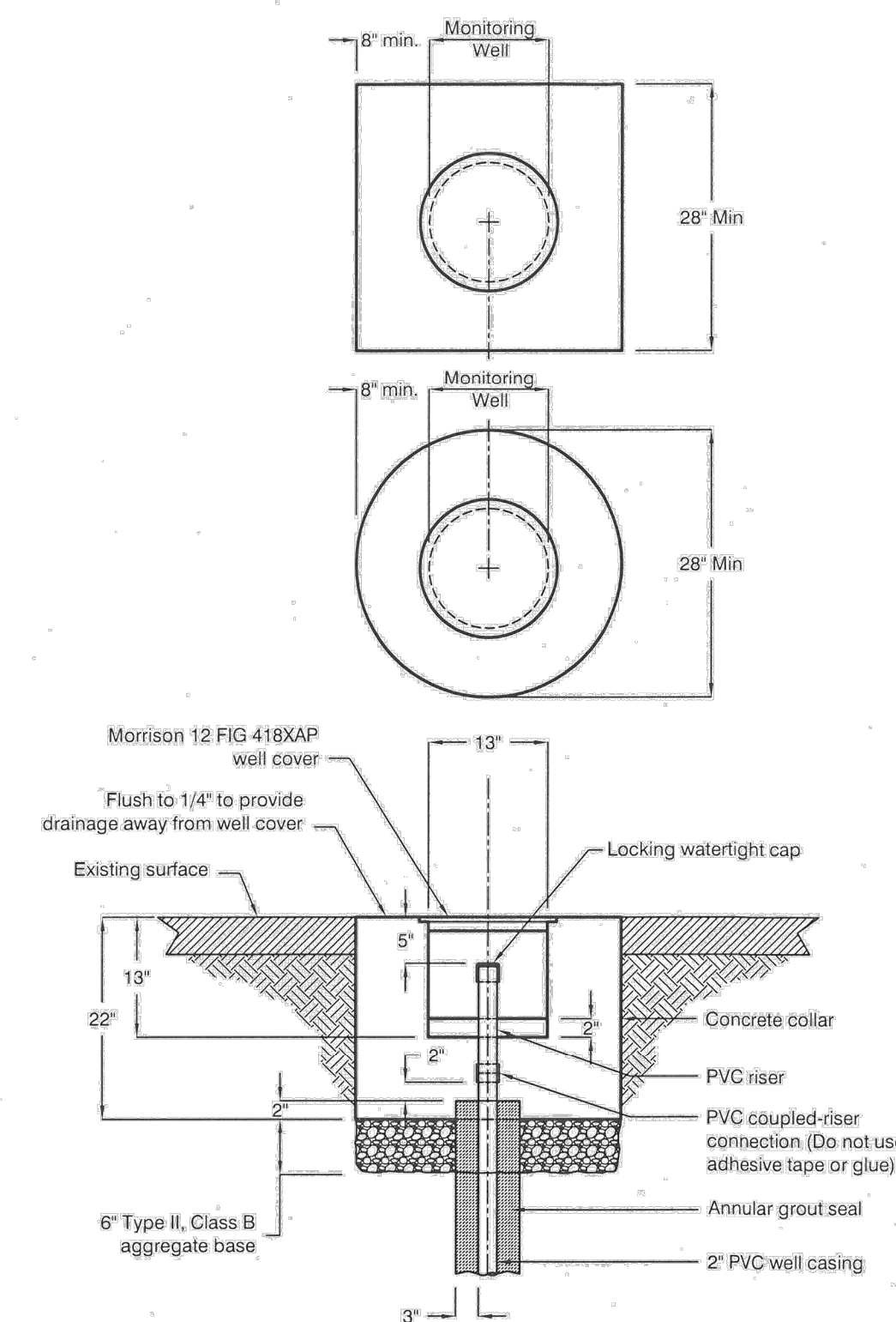
Washoe County
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4930 Energy Way
Reno, NV 89502-4106
Tel: (775) 954-4600
Fax: (775) 954-4610
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Well reconstruction

- Contact Tim Donahoe (954-4635/544-3826) before reconstructing and raising a monitoring well to provide oversight of the process.
- Excavate around the well to a circular diameter of at least 28" and a depth of 28". Square-shape excavation is not recommended unless required by logistics of site.
- Place 6" of Type 2, Class B aggregate base at the base of the excavation and compact manually.
- Add the PVC riser pipe by dry-fitting a PVC coupling. Use either the piece of pipe that was cut off and saved or an ASTM F480 pipe which matches the schedule and diameter of the buried pipe. Do not use any glue or adhesive!
- Pour concrete to cover the road base, grout seal around the pipe, and PVC joint.
- Set the Morrison manhole/valve box so that the lip is no more than 1/4" above the final grade of the road. The triangle on the lid should point North.
- Check the final dimensions (Fig. 3) and complete the apron with a broom finish.

*Do not use any PVC glue, primer, or any type of adhesive on the coupling!



Notes:
The completed vault will have a well cover that is centered within the concrete collar and is flush or extends no more than one-quarter inch above the roadway surface. The concrete apron will be graded such that the well cover rim is flush with the concrete and the apron slopes away from the well cover to provide drainage away from the well cover creating a watertight configuration. Well cover and concrete collar shall be appropriately traffic-rated and meet City or County specifications.

WCDWR Monitoring Well Vault specifications

Figure 3

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ORIGINAL SHEET - ANSI D

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DT-2

WASHOE COUNTY MONITORING WELL
ADJUSTMENT DETAIL

ISSUED FOR BID

Client/Project
CITY OF SPARKS

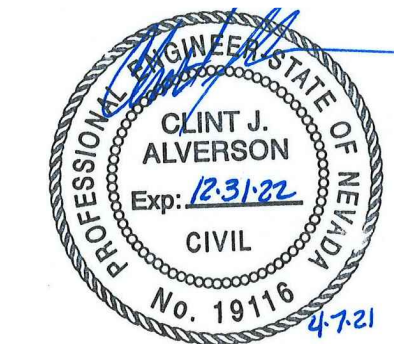
2021 PCC ALLEYS

Sparks, NV

Title

DETAILS

Permit-Seal



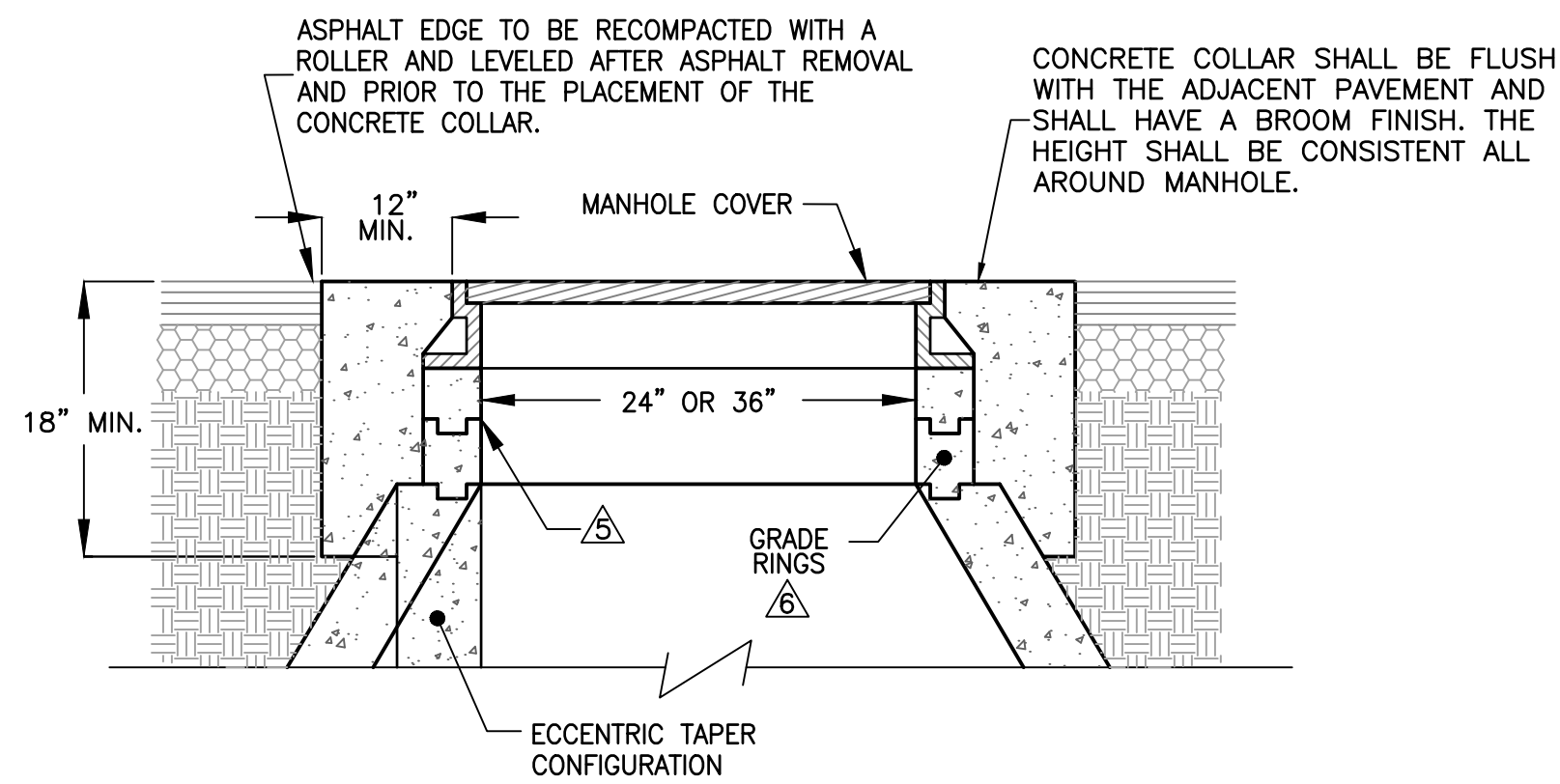
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Drawing No. DT-2

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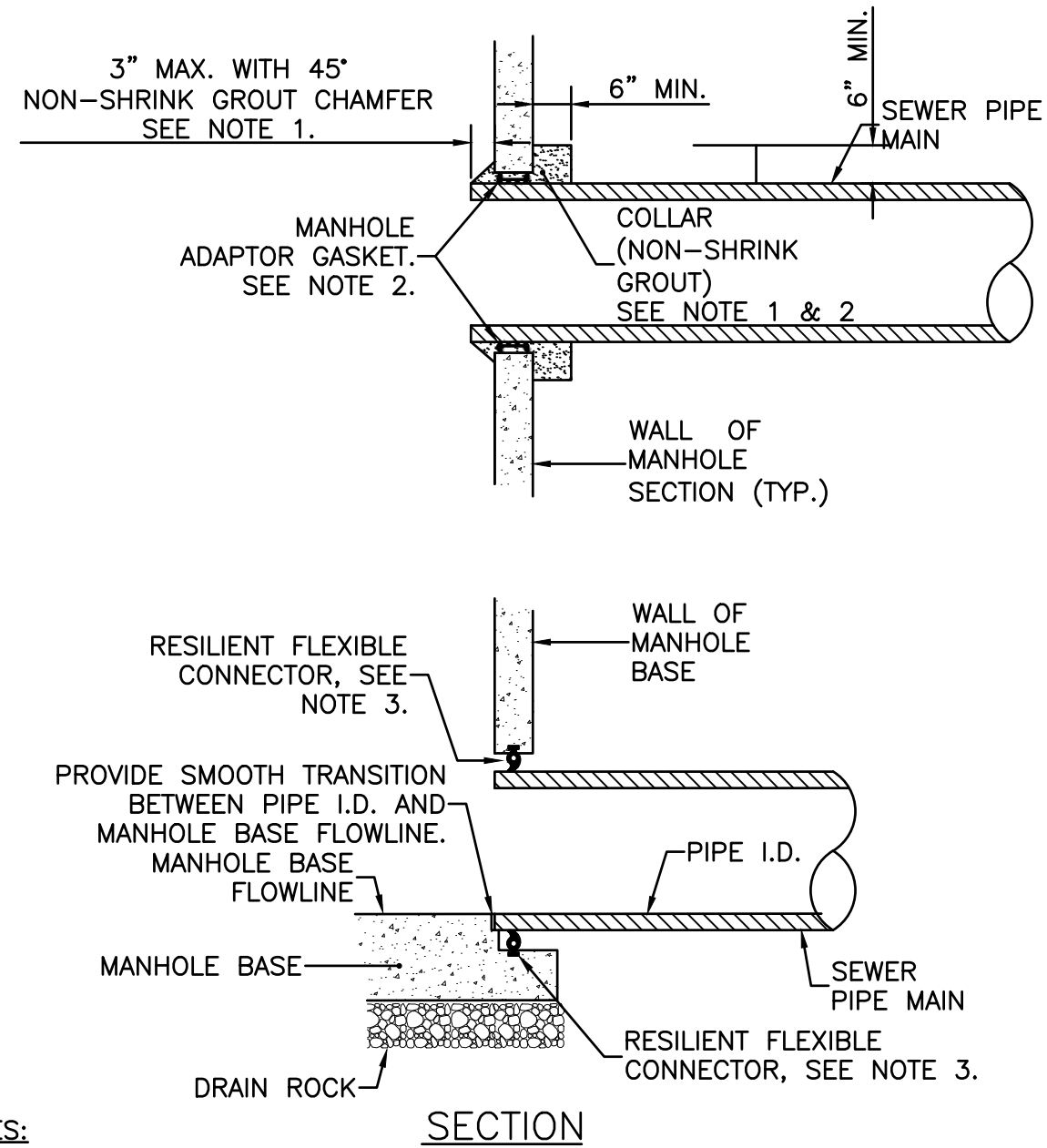
0 11 of 12



NOTES:

- SEE SPECIFICATIONS FOR CONCRETE MIX DESIGN.
- CIRCUMSTANCES MAY REQUIRE THE NEED FOR SPECIAL TYPES OF TOP OF MANHOLE CONFIGURATIONS SUCH AS FLAT TOP ABOVE GROUND, ETC. AS DIRECTED BY THE OWNING AGENCY. DETAILED PLANS OF ANY SPECIAL TOP OF MANHOLE CONFIGURATIONS AND ASSOCIATED COLLARS MUST BE APPROVED BY THE ENGINEER.
- IN UNPAVED AREAS, IT SHALL BE NECESSARY TO SET THE MANHOLE RIM APPROXIMATELY 6 INCHES ABOVE THE SURROUNDING AREA. INSTALL A 6 INCH THICK RING OF CONCRETE, TAPERED AT A 3:1 SLOPE, FROM THE TOP, OUTSIDE EDGE OF THE COLLAR TO THE EXISTING GROUND SURFACE.
- MANHOLE LIDS SHALL NOT BE LOCATED IN GUTTER PANS, UNLESS OTHERWISE APPROVED BY THE OWNING AGENCY.
- ALL GRADE RING JOINTS ARE TO BE GROUTED WITH NON-SHRINK GROUT CONFORMING TO SSPWC SECTION 202 "CEMENT AND RELATED MATERIALS."
- ALL GRADE RINGS SHALL BE PORTLAND CEMENT CONCRETE. PVC GRADE RINGS ARE NOT ALLOWED.

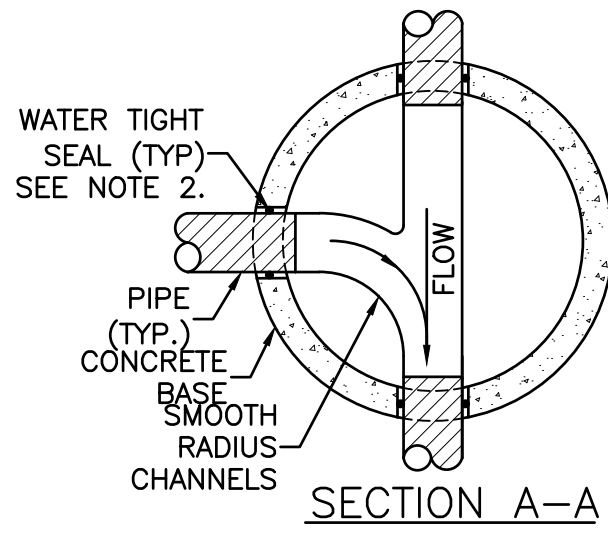
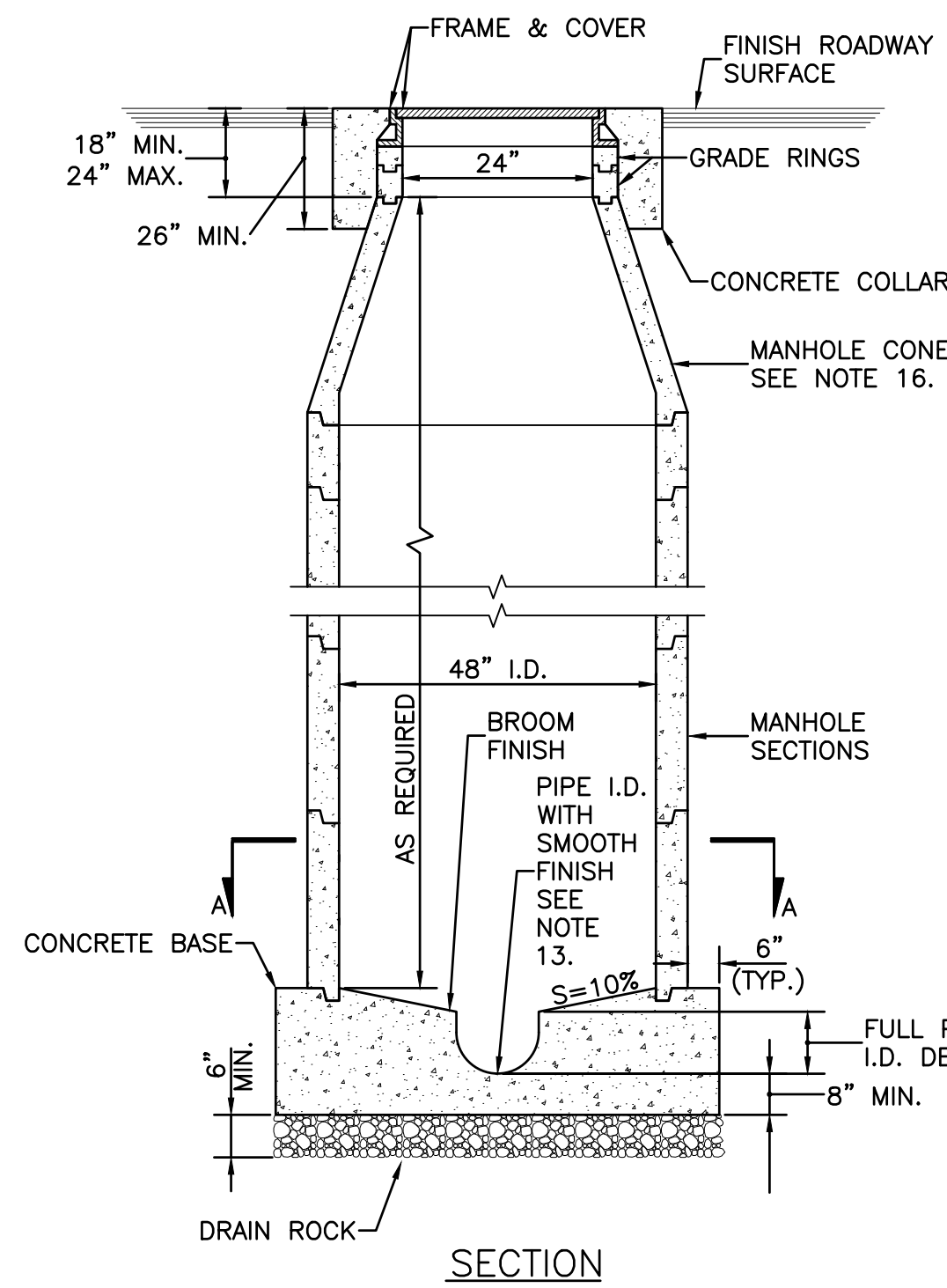
1 ADJUSTMENT MANHOLE TO FINISHED GRADE
DT-3



NOTES:

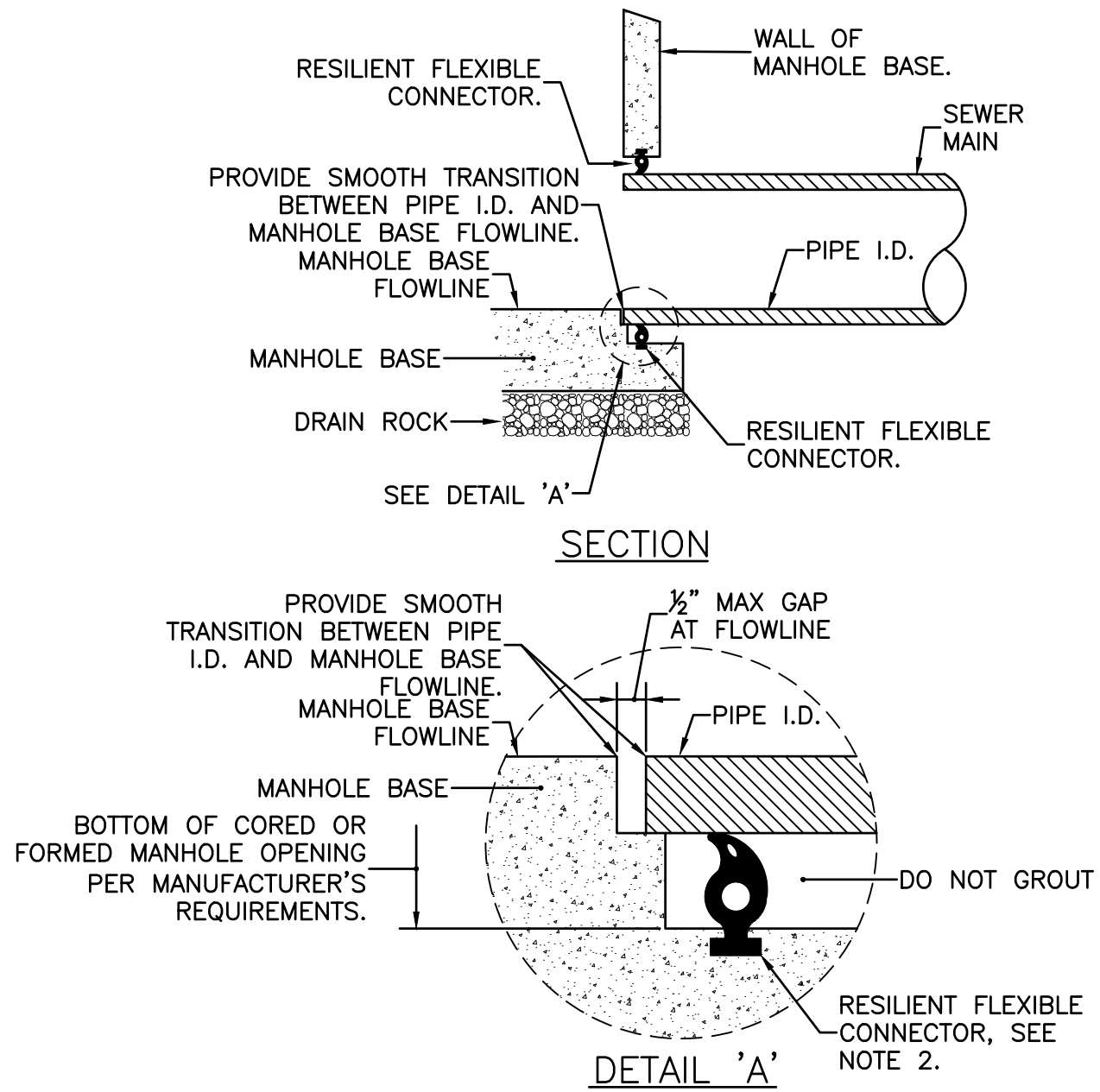
- NON-SHRINK GROUT SHALL HAVE THE FOLLOWING CHARACTERISTICS: 3000 PSI MIN. COMPRESSIVE STRENGTH AT 28 DAYS, MIN. 6 SACKS OF CEMENT PER CUBIC YARD AND SLUMP AT 1 TO 4 INCHES. ALL MATERIAL SHALL CONFORM TO STANDARD SPECIFICATIONS OF PUBLIC WORKS CONSTRUCTION (SSPWC) SECTION 202, AS ADOPTED BY CITY COUNCIL.
- SANITARY SEWER PIPE CONNECTION TO MANHOLE SECTIONS (DROP MANHOLES) REQUIRE AN AGENCY-APPROVED FORM OF SEAL OR WATER STOP TO PROVIDE A WATERTIGHT CONNECTION. UTILIZE A ROMAC STYLE "LCT" MANHOLE ADAPTOR GASKET OR APPROVED EQUAL IN CONJUNCTION WITH NON-SHRINK GROUT.
- SANITARY SEWER PIPE CONNECTION TO MANHOLE BASE SHALL REQUIRE A RESILIENT FLEXIBLE CONNECTOR INSTALLED IN ACCORDANCE WITH STANDARD DETAIL S-211C.
- ALL PIPE OPENINGS TO NEW MANHOLES MUST BE EITHER CAST-IN-PLACE OR PRE-FORMED AND PIPE OPENINGS TO EXISTING MANHOLES MUST BE CORE DRILLED.

3 SANITARY SEWER PIPE TO MANHOLE CONNECTION
DT-3



SECTION

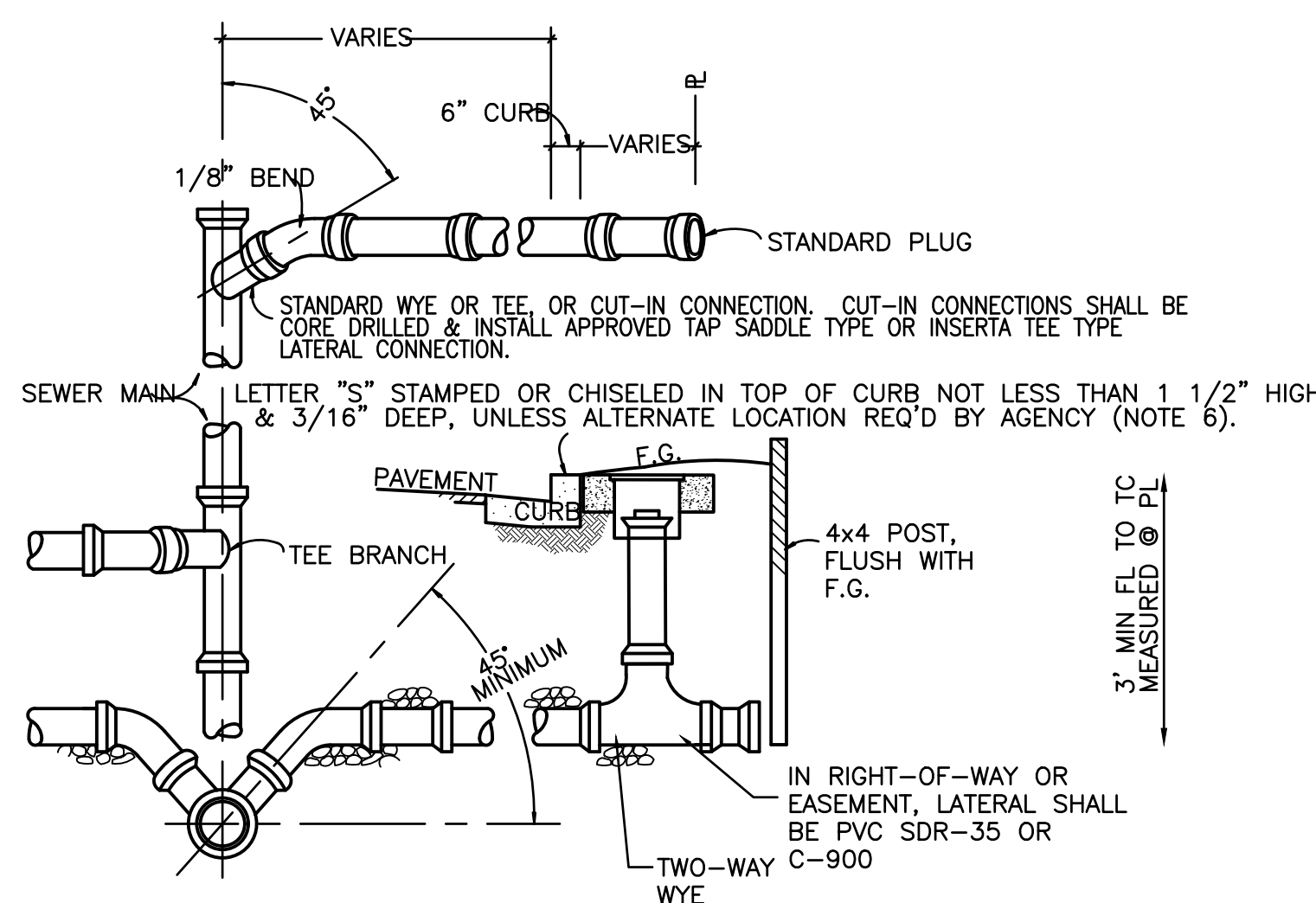
2 TYPE I MANHOLE
DT-3



NOTES:

- A SEAL OR WATER STOP IS REQUIRED ON ALL SANITARY SEWER INSTALLATIONS AND IN OTHER APPLICATIONS AS REQUIRED BY THE CITY TO PROVIDE A WATERTIGHT CONNECTION.
- A RESILIENT FLEXIBLE CONNECTOR PER ASTM C 923-89 SHALL BE USED AT THE MANHOLE/PIPE CONNECTION TO SATISFY THE REQUIREMENTS OF NOTE 1. FOR PRE-CAST CONCRETE STRUCTURES, THE RESILIENT FLEXIBLE CONNECTOR SHALL BE AN "A-LOK" TYPE PIPE-TO-MANHOLE CONNECTOR OR APPROVED EQUAL. FOR CAST-IN-PLACE STRUCTURES, THE RESILIENT FLEXIBLE CONNECTOR SHALL BE A "KOR-N-SEAL I - TOGGLE KOR-BAND" TYPE PIPE-TO-MANHOLE CONNECTOR OR APPROVED EQUAL.
- THE INTERIOR MANHOLE CONNECTION SHALL HAVE A SMOOTH TRANSITION BETWEEN PIPE I.D. AND MANHOLE BASE FLOWLINE. NO GROUT OR CONCRETE SHALL BE PLACED AROUND THE RESILIENT FLEXIBLE CONNECTOR.

4 RESILIENT FLEXIBLE CONNECTOR
DT-3



NOTES:

- SANITARY SEWER LATERALS SHALL HAVE A MINIMUM SLOPE OF 2%, UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER.
- LATERAL SHALL BE CUT BACK TO SOUND MATERIAL FOR COUPLING.
- ALL CONNECTIONS TO THE CITY SEWER MAINS MUST BE CORE DRILLED.
- ENCASE LATERAL CONNECTION IN CEMENT, STABILIZED SAND OR 2000 PSI CONCRETE FOLLOWING INSTALLATION. CONNECTION MUST BE INSPECTED BY CITY PRIOR TO BACKFILL.
- NO LATERAL CONNECTIONS SHALL BE MADE TO SANITARY SEWER "INTERCEPTOR" LINES WITHOUT THE APPROVAL OF THE CITY ENGINEER.
- ALL APPLICABLE SANITARY SEWER PIPE INSTALLATION WORK SHALL BE IN ACCORDANCE WITH THE UNIFORM PLUMBING CODE.

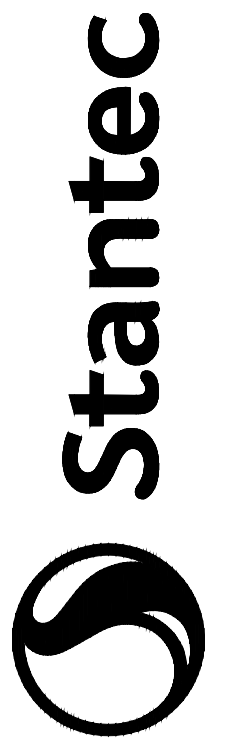
5 SANITARY SEWER LATERAL
DT-3

GENERAL MANHOLE NOTES

- ALL PRECAST MANHOLE COMPONENTS SHALL CONFORM TO ASTM C-478.
- PIPES SHALL NOT PROTRUDE MORE THAN 3" INSIDE MANHOLE SECTION AS MEASURED AT THE OUTSIDE EDGES OF THE PIPE, VERTICALLY ALIGNED WITH THE SPRINGLINE. PIPE CONNECTION TO MANHOLE SHALL BE WATERTIGHT PER STANDARD DETAILS S-211A, S-211B AND S-211C.
- MANHOLE BASE SHALL BE PORTLAND CEMENT CONCRETE (P.C.C.) AND SHALL HAVE THE FOLLOWING CHARACTERISTICS: 3000 PSI MINIMUM COMPRESSIVE STRENGTH AT 28 DAYS, MINIMUM 6 SACKS OF CEMENT PER CUBIC YARD WITH SLUMP AT 1 TO 4 INCHES. ALL MATERIAL SHALL CONFORM TO STANDARD SPECIFICATIONS OF PUBLIC WORKS CONSTRUCTION (SSPWC), AS ADOPTED BY CITY COUNCIL. PRECAST CONCRETE BASE MAY BE USED IN LIEU OF CAST-IN-PLACE BASE.
- TYPE I MANHOLE TO BE UTILIZED FOR PIPE DIAMETERS OF 12" OR SMALLER AND DEPTHS NOT EXCEEDING 18 FEET.
- TYPE V MANHOLE TO BE UTILIZED FOR PIPE DIAMETERS OF 15" THROUGH 27" OR DEPTHS EXCEEDING 18 FEET.
- MANHOLE MATERIALS AND CONSTRUCTION SHALL CONFORM TO THE REQUIREMENTS OF SECTION 204 "MANHOLES AND CATCH BASINS" OF THE STANDARD SPECIFICATIONS.
- PRECAST MANHOLE SECTIONS, OTHER THAN GRADE RINGS, SHALL BE JOINED WITH FLEXIBLE GASKET MATERIAL SUCH AS "RAM-NEK" OR EQUAL AS PER MANUFACTURER'S RECOMMENDATIONS.
- EXCAVATION AND BACKFILL SHALL BE AS SPECIFIED FOR "TRENCH EXCAVATION AND BACKFILL" IN SECTION 305 OF THE STANDARD SPECIFICATIONS.
- EXCAVATION SHALL BE AS NEARLY VERTICAL AS POSSIBLE (SHEET AND SHORE IF SOIL CONDITIONS REQUIRE) IN EXISTING STREET SECTIONS, ALLEY SECTIONS, AND CONFINED AREAS, SUCH AS LIMITED EASEMENTS OR ADJACENT STRUCTURES.
- MANHOLE PRECAST SECTION LENGTH SHALL BE ARRANGED TO FIT THE REQUIRED DEPTH.
- NO LATERALS OR PIPES LESS THAN 8" IN DIAMETER SHALL BE CONNECTED TO THE MANHOLE.
- PRECAST CONCRETE BASE MAY BE USED IN LIEU OF CAST-IN-PLACE BASE.
- MATCH PIPE INVERTS TO MANHOLE INVERTS WHERE PIPES CONNECT TO MANHOLE BASE.
- ALL MANHOLES SHALL BE WATERTIGHT.
- SEE DETAIL FOR INSIDE DROP MANHOLE FOR SANITARY SEWERS WITH MORE THAN 2 FEET VERTICAL DROP AT THE MANHOLE. THE USE OF "OUTSIDE DROP" MANHOLES IS NOT PERMITTED.
- THE USE OF FLAT TOP MANHOLE CONES REQUIRES PRIOR APPROVAL FROM THE CITY ENGINEER.
- PRIOR TO BACKFILLING, ALL MANHOLES SHALL BE VACUUM TESTED PER ASTM C-1244.
- NO STEPS, LADDERS, OR OTHER CLIMBING DEVICES SHALL BE INSTALLED IN THE MANHOLE.
- REINFORCING STEEL SHALL BE AS SHOWN, WIRED TIGHTLY AT ALL INTERSECTIONS AND EMBEDDED AT LEAST 1 1/2" CLEAR, UNLESS OTHERWISE NOTED.
- WHEN PIPE CONNECTIONS TO EXISTING MANHOLES ARE ALLOWED, THEY SHALL BE MADE BY CORE DRILLING THE MANHOLE AND CONNECTING THE PIPE PENETRATION PER DETAIL S-211A & S-211B.
- TYPE IV MANHOLES SHOULD ONLY BE USED AS APPROVED BY CITY ENGINEER

NOTES:

- SEWER LATERALS SHALL HAVE A MINIMUM PIPE DIAMETER OF 4-INCHES.
- ALL PLASTIC PIPE USED FOR SEWER SERVICE LATERAL CONSTRUCTION SHALL BE SOLID WALL AND SHALL MEET THE REQUIREMENTS OF D-2412. HAVE A MINIMUM STIFFNESS OF 46 PSI AS DEFINED BY THE REQUIREMENTS OF ASTM D-3034.
- SERVICE LATERALS SHALL HAVE A MINIMUM SLOPE OF 1/4-INCH PER FOOT UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER.
- SEWER LATERALS SHALL HAVE A MINIMUM COVER OF 36-INCHES IN THE PUBLIC RIGHT-OF-WAY AND IN EASEMENTS UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER. THE DEFINITION OF "COVER" IS THE DISTANCE FROM THE TOP OF PIPE TO FINISHED GRADE.
- USE OF SEWER SERVICE TAP SADDLE CONNECTIONS SHALL NOT BE ALLOWED FOR NEW SEWER MAIN CONSTRUCTION. WHEN A TAP SADDLE CONNECTION IS TO BE USED ON AN EXISTING SEWER MAIN, IT SHALL BE A WYE SADDLE AND BE INSTALLED PER DETAIL S-212C.
- SEWER LATERALS SHALL HAVE A CLEANOUT INSTALLED BETWEEN SIDEWALK AND RIGHT-OF-WAY. A G5 BOX CLEARLY MARKED "SEWER" SHALL BE INSTALLED OVER THE TOP OF THE CLEANOUT RISER CAP PIPE.
- SEWER LATERALS SHALL NOT BE CONNECTED DIRECTLY TO OR WITHIN 5- FEET OF A MANHOLE STRUCTURE.
- EXISTING SEWER LATERALS SHALL BE CUT BACK TO SOUND MATERIAL FOR COUPLING. PLACE 6-INCH THICK CONCRETE PAD UNDER CONNECTION.
- SEWER LATERAL CONNECTION SHALL BE STABILIZED WITH APPROVED MATERIAL FOLLOWING INSTALLATION. CONNECTION TO CITY SEWER MAIN MUST BE INSPECTED BY THE CITY PRIOR TO BACKFILL.
- NO LATERAL CONNECTIONS SHALL BE MADE DIRECTLY TO A SANITARY SEWER "INTERCEPTOR" UNLESS APPROVED BY THE CITY ENGINEER.
- SEWER LATERALS SHALL NOT BE CONNECTED TO A SEWER MAIN UNLESS THE CONNECTION POINT IS BETWEEN TWO MANHOLE STRUCTURES.
- EACH INDIVIDUAL PARCEL SHALL HAVE A MINIMUM OF ONE SEWER LATERAL. TWO OR MORE PARCELS SHALL NOT SHARE ONE SEWER LATERAL.
- SANITARY SEWER LATERAL IDENTIFICATION AND LOCATING REQUIREMENTS SHALL BE PER SPARKS MUNICIPAL CODE 17.16.130 (SMC 17.16.130).
- DISCONTINUANCE OF USE OF AN EXISTING SEWER LATERAL REQUIRES ABANDONMENT OF THE LATERAL. CUT, REMOVE 1-FOOT OF EXISTING LATERAL AND CAP BOTH ENDS OF THE EXISTING SEWER LATERAL TO BE ABANDONED WITHIN 6-INCHES OF THE SEWER MAIN. ABANDONMENT MUST BE INSPECTED BY CITY PRIOR TO BACKFILL.
- PROPERTY OWNER SHALL BE RESPONSIBLE FOR OPERATION, MAINTENANCE AND REPAIR OF THE SEWER LATERAL WITHIN THE PUBLIC RIGHT-OF-WAY PER SPARKS MUNICIPAL CODE.



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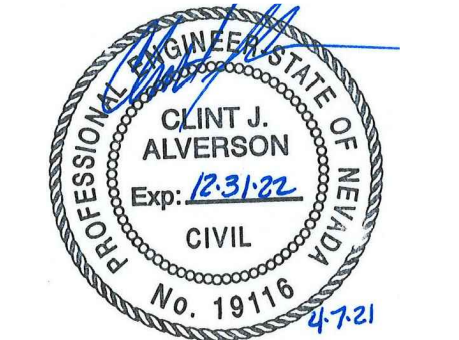
Revision	By	Appd.	Y1MMDD
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Client/Project
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2021 PCC ALLEYS

Sparks, NV
Title
DETAILS



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